



Level 3, 124 Exhibition Street
[GPO Box 2634, Melbourne 3001]
Melbourne, Victoria 3000

enquiries@gjmheritage.com

+61 (03) 9115 6566

gjmheritage.com

ABN: 62 348 237 636

ARBV: 16044

BUILT FORM REVIEW:

VICTORIA PARADE

HERITAGE ANALYSIS & RECOMMENDATIONS



PREPARED FOR: City of Yarra

DATE: November 2020

FILE: 2020-008

This report is limited in its scope to consideration of post-contact cultural heritage and does not provide advice on any Aboriginal cultural heritage significance. The subject site forms part of the 37.5% of Victoria where there is currently no formally recognized Traditional Owner groups. Nonetheless, we acknowledge the Traditional Owners of the land at this place and pay our respects to their Elders past, present and emerging. For more information, please visit <https://www.aboriginalvictoria.vic.gov.au>

PROJECT TEAM

Jim Gard'ner | Director

Renae Jarman | Director

Ros Coleman | Associate

Jessica Hogg | Heritage Consultant

Cover Image: *the south side of Victoria Parade looking east from the intersection with Napier Street*

GLOSSARY OF ABBREVIATIONS

Appendix 8	Incorporated Document 'City of Yarra Review of Heritage Overlay Areas 2007: Appendix 8 (revised May 2018)'
C1Z	Commercial 1 Zone
C2Z	Commercial 2 Zone
DDO	Design and Development Overlay
DELWP	Department of Environment, Land, Water and Planning
GRZ	General Residential Zone
HO	Heritage Overlay
MAC	Major Activity Centre
MUZ	Mixed Use Zone
NAC	Neighbourhood Activity Centre
NRZ	Neighbourhood Residential Zone
PPN	Planning Practice Note
VHR	Victorian Heritage Register

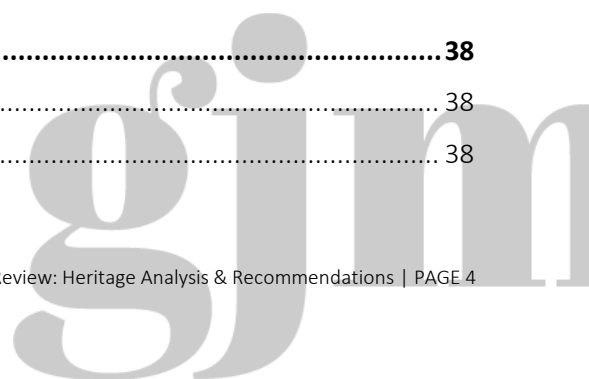
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EXECUTIVE SUMMARY

Hansen Partnership, on behalf of the City of Yarra (Council), has prepared a Built Form Review of three precincts: Alexandra Parade, Victoria Parade and Fitzroy West. This forms part of a larger project that considers built form controls for commercial high streets and mixed use / commercial zoned precincts within Fitzroy, Collingwood and (part) Clifton Hill (Figure 1). The purpose of this Built Form Review work is to determine where and how new development can appropriately occur. The desired built form outcomes will be translated into Design and Development Overlay (DDO) controls for the study area.

This report specifically considers the Victoria Parade Precinct (also referred to as the 'study area' in this report) which includes land within the southern part of Collingwood and Fitzroy. The heritage advice contained within this report will help ensure that the Built Form Review and the subsequent DDO controls appropriately respond to the heritage fabric and values of the study area.

This advice then considers the built form parameters that are required to ensure the values of heritage places within the Victoria Parade Precinct are appropriately managed and protected, and that good heritage outcomes are being achieved for potential new development or redevelopment on land subject to, or abutting, the Heritage Overlay.

Part II of this report provides an analysis of gaps, inconsistencies and inaccuracies in the current heritage controls within the study area and provides recommendations for addressing these issues. This has resulted in the assessment of one potential heritage place, a row of late nineteenth century shop/residences at 205-219 Victoria Parade, Collingwood.

This Heritage Analysis and Recommendation Report is presented in three parts:

Part I: The Project and Planning Framework

Part I introduces the project, the methodology applied to the project and the planning framework in which the project is occurring.

Part II: Heritage Analysis

Part II contains a heritage analysis of the study area. It details the heritage qualities and values of each precinct, identifies any gaps or issues in the existing heritage framework and provides recommendations for appropriately managing heritage places within the study area.

Part III: Built Form Recommendations

Part III contains specific built form recommendations to ensure heritage places and values are appropriately managed within a changing urban context. The specific recommendations are informed by modelling prepared by Hansen Partnership.



Figure 1: Brunswick and Smith Street Study Area
 Source: 'Figure 2: Brunswick & Smith Street Built Form Review - Precincts' from the Victoria Parade Built Form Framework (Hansen Partnership, December 2020).

The Victoria Parade Precinct is denoted by the 'V' and is dark green in colour.

Figure 2

Legend	Activity Spines	Mixed Use Precincts	Boulevard Precincts
study area	brunswick street	fitzroy west	alexandra parade
precinct boundary	smith street	fitzroy east	victoria parade
	gertrude street	collingwood	
	johnston street	town hall	

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hansen partnership ply ltd
 melbourne | vietnam
 level 4 | 538 exhibition st
 melbourne vic 3000
 t 61 3 9654 9044
 f 61 3 9654 8088
 info@hansenpartnership.com.au
 hansenpartnership.com.au



PART I: THE PROJECT AND PLANNING FRAMEWORK



1.0 INTRODUCTION

1.1 YARRA'S HIGH STREETS

The City of Yarra is endowed with one of the largest and most highly intact collections of turn of the century 'High Streets' in the State of Victoria. These High Streets include the Major Activity Centres of Swan Street and Bridge Road in Richmond, Brunswick Street in Fitzroy, Smith Street straddling the suburbs of Fitzroy and Collingwood and Victoria Street, in Abbotsford and Richmond. They also include a number of Neighbourhood Activity Centres, including Gertrude Street in Fitzroy, Johnston Street in Fitzroy & Collingwood, Rathdowne Street and Nicholson Street in Carlton North, St Georges Road in Fitzroy North, and Queens Parade in Fitzroy North & Clifton Hill.

These High Streets contrast with the wide boulevards of Alexandra and Victoria Parade, both of which are now multi-lane major arterial routes. These boulevards both retain their wide median strips. Unlike the more typical commercial High Street, these boulevards do not exhibit a consistency of use or built form. Nonetheless, while the built form, character and heritage values of these boulevards differ greatly over their length, the pockets of heritage buildings warrant special consideration as do the interfaces to the generally low-scale residential heritage areas they directly abut. Like the historic High Streets, it is necessary to manage the tension between the desire to retain the heritage values of these areas and meet the growth objectives of the Yarra Planning Scheme.

1.2 BRIEF DESCRIPTION OF THE VICTORIA PARADE STUDY AREA

Victoria Parade is a major arterial road running east-west connecting to Victoria Street at Nicholson Street to the west and Hoddle Street to the east. Victoria Parade forms the boundary between the City of Melbourne to the south and the City of Yarra to the north. It has a 60m wide carriageway with three lanes of traffic (plus a bus lane) running in each direction. A light-rail tram line runs within the centre of the 35m wide grassed median strip that separates the northern and southern carriageways. The median is planted with a mature avenue of Dutch Elms. Street trees are planted less regularly along the northern and southern footpaths. The north and south oriented roads do not generally align across Victoria Parade.

The study area extends along the north side of Victoria Parade from Napier Street in the west to Hoddle Street in the east, generally to the depth of one property (noting that individual properties range in depth from as little as 20m to over 100m).

The built form of the Victoria Parade Precinct is highly varied ranging from large, low-rise late twentieth century offices and showrooms towards the eastern end to highly intact turn of the century shop/residences and terraced housing at the western end, including the State heritage listed Blanche Terrace at 169-179 Victoria Parade, Fitzroy (VHR H0177-H0182) and McClelland House at 203 Victoria Parade, Fitzroy (VHR H0590). Beyond the heritage-listed shop/residences and terraced housing at the western end of the precinct, and the small run of un-listed nineteenth century shop/residences between Islington and Hoddle Streets, there is little visual

cohesion within the precinct. Approximately half the length of the precinct as it addresses Victoria Parade is (intermittently) subject to the Heritage Overlay.

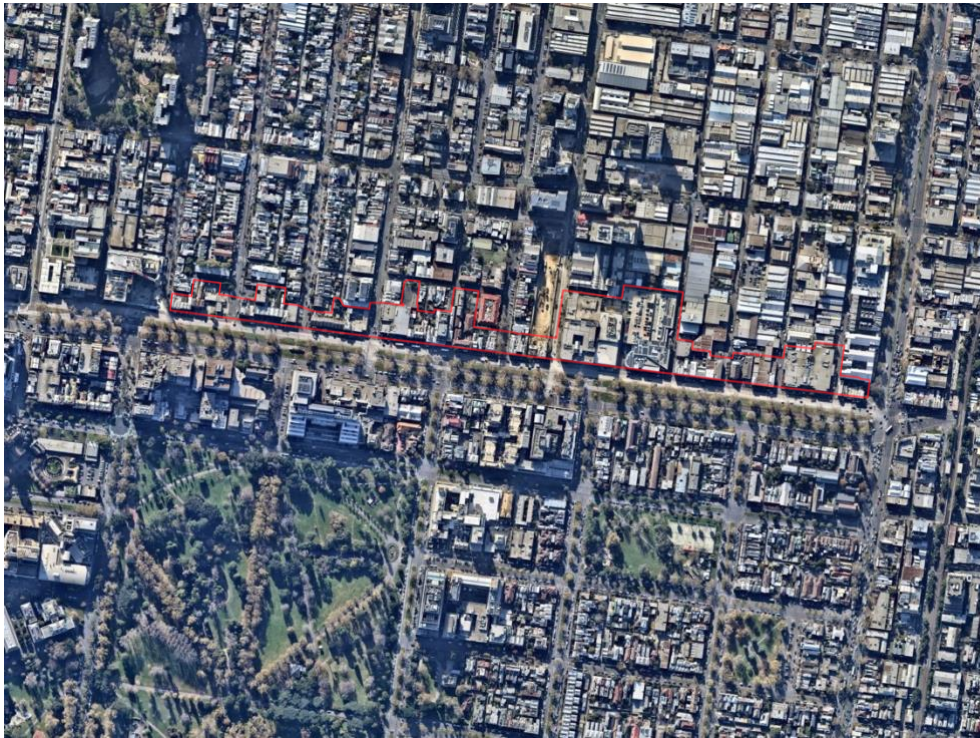


Figure 2: Victoria Parade precinct – outlined in red.

Source: Nearmap, 4 June 2020

1.3 BRIEF HISTORY OF VICTORIA PARADE

This historical summary is based on the City of Yarra Heritage Review Thematic History (Allom Lovell & Associates, 1998) with additional sources as cited.

Victoria Parade was evident on James Kearney's 1855 map of Melbourne and its suburbs, extending between Nicholson Street and Hoddle Street, and marking the boundary between the early suburbs of East Melbourne to the south and Fitzroy (originally called Newtown) and Collingwood to the north.

The western end of Victoria Parade rises to what was originally called Eastern Hill. This elevated area was where many of Melbourne's wealthier and more influential early residents settled and built fine, often architect-designed houses (Lewis 1989:21). Many residences, medium to large in scale, were built fronting Victoria Parade (particularly the western end) from the 1860s and throughout the Victorian period, often in the terrace form which dominated Fitzroy from the 1850s (Lewis 1989:21). Evidence of this remains within the study area at Blanche Terrace (1867; 169-179 Victoria Parade, Fitzroy), Russell House (1862; 181 Victoria Parade, Fitzroy), Alexandra Terrace (1864-65; 145-149 Victoria Parade, Fitzroy), the pair of terrace houses at 163-165 Victoria Parade, Fitzroy (1868), and Irwell Terrace (1868; 19-23 Victoria Parade, Collingwood). These residences within the western portion of the study area were located amongst a small number of doctors, businesses and trades in the 1860s (S&Mc).



Figure 3: Looking east down Victoria Parade, showing Fitzroy and Collingwood to the left. Photo dated c1905-c1910 (COYL, Image CL PIC 558).



Figure 4: Looking east down Victoria Parade, near the corner of Cambridge Street, Collingwood. Photo dated c1906-c1918 (COYL, Image CL PIC 594)

Down the hill, the eastern portion of Victoria Parade was largely developed by 1858, occupied by buildings of various form and scale, as evident on Clement Hodgkinson’s 1858 map of East Collingwood. The 1860 Sands & McDougall Directory indicates that this part of the study area, east of Wellington Street, comprised a high number of commercial buildings and a majority of occupants practicing trades – carpenters, butchers, plumbers, bricklayers, carters and bootmakers.

The proceeding decades saw the erection of the former Ebenezer Baptist Church (1870; 159 Victoria Parade, Collingwood) and further residences such as Floraston (1876; 39 Victoria Parade, Collingwood), McLelland House Terrace (1882; 203 Victoria Parade, Fitzroy) and Portia (1889; 15-17 Victoria Parade, Collingwood) within the study area. Commercial endeavours of the 1880s included the Prince Patrick Hotel and shops (1887; 135-141 Victoria Parade, Collingwood) and the



former John Franklin Hotel (1880s; 75a-77 Victoria Parade, Collingwood). Some light industrial development occurred immediately north of Victoria Parade including the former Smalley & Harkness Boot Factory (1898; 16 Islington Street, Collingwood). A tramline was established along Victoria Parade in the 1880s (and a new electrified line with overhead tram poles was constructed down the centre of the road reserve in the 1920s) (Vines 2011:24).

While the precinct was predominantly developed during the Victorian period, some construction occurred in the first half of the twentieth century. An example is the former Melbourne After Care Home (1926, 1936; 45-47 Victoria Parade, Collingwood), built during the Interwar period. Since the 1970s, the eastern portion of the precinct towards Hoddle Street has seen an increase in development, with a mix of residential, and often large-scale, commercial and office buildings erected.

Sources

Allom Lovell & Associates (1998), *City of Yarra Heritage Review, Thematic History*.

City of Yarra Libraries (COYL), online picture collection.

Hodgkinson, Clement, Noone, John, and Wilkinson, John, (1858), *Plan shewing the streets and buildings in existence in East Collingwood on January 1st 1858 : with schedule of heights of bench-marks above low water datum at Queen's wharf* [cartographic material]. Printed by De Gruchy & Leigh [Melbourne].

Kearney, James (draughtsman), Brown, James D. and Tulloch, David (engravers) (1855), *Melbourne and its suburbs* [cartographic material].

Lewis, Miles (1989) 'The First Suburb', in Fitzroy History Society & Cutten History Committee, *Fitzroy : Melbourne's first suburb*, South Yarra [Vic].

Sands & McDougall Directory (S&Mc).

Vines, Gary (2011), *Melbourne Metropolitan Tramways Study, Heritage Places*

1.4 SCOPE OF THE HERITAGE ANALYSIS & RECOMMENDATIONS REPORT

GJM Heritage has been commissioned to provide a detailed analysis of the heritage considerations for the Brunswick and Smith Street Built Form Review project area and to detail recommendations for the future management of these areas in the context of potential new development. This report considers the Victoria Parade Precinct and has been prepared simultaneously with those considering the Alexandra Parade and Fitzroy West Mixed Use precincts.

The following precincts have previously been considered in the *Brunswick and Smith Street Built Form Review*, GJM Heritage, 25 November 2019:

- Brunswick Street Activity Centre Spine
- Town Hall Mixed Use Precinct
- Smith Street Activity Centre Spine
- Johnston Street Activity Centre Spine
- Fitzroy East Mixed Use Precinct.

The proposed Gertrude Street Precinct and the MUZ area south of Gertrude Street between Young and Little Napier Streets was reviewed through the *Gertrude Street Built Form Framework: Heritage Analysis & Recommendations*, GJM Heritage, 9 December 2019.

The Collingwood Mixed Use Precinct was considered as part of a separate study: *Collingwood Mixed Use Pocket Heritage Analysis & Recommendations*, GJM Heritage, 6 June 2018.

The purpose of our advice as part of this project is to ensure that any DDO controls arising from the Built Form Review take proper account of the heritage values of the precincts and individual buildings within the study area, in order to ensure appropriate weight is given to heritage when considering new development within the Victoria Parade Precinct.

The analysis within this report builds on the previous built form reviews and heritage analysis work conducted within the City of Yarra, and considers the parameters necessary to appropriately manage increased commercial and residential development within the Victoria Parade Precinct.

1.5 METHODOLOGY

The key background documents on which the heritage analysis is based are:

- Yarra Planning Scheme Heritage Overlay Maps 5 HO and 6HO
- Relevant Statements of Significance for heritage places and precincts within the study area and associated heritage studies
- Incorporated Document 'City of Yarra Review of Heritage Overlay Areas 2007: Appendix 8 (revised May 2018)' (Appendix 8)
- City of Yarra Heritage Grading Maps

The above documents have been reviewed in the context of the following clauses of the Yarra Planning Scheme and the relevant Planning Practice Notes (PPNs) published by the Department of Environment, Land, Water and Planning (DELWP):

- The relevant provisions of the Yarra Planning Scheme, in particular, are:
 - Clause 15.03-1S 'Heritage conservation'
 - Clause 21.05-1 'Heritage'
 - Clause 22.02 'Development Guidelines for Sites Subject to the Heritage Overlay'
 - Clause 22.10 'Built Form and Design Policy'
 - Clause 43.01 'Heritage Overlay'
 - Clause 43.01 'Schedule to the Heritage Overlay'
 - Clause 71.02-3 'Integrated Decision Making'
- PPN 1: *Applying the Heritage Overlay* (August 2018) (PPN1)
- PPN 59: *The role of mandatory provisions in the planning schemes* (September 2018) (PPN59)
- PPN 60: *Height and setback controls for activity centres* (September 2018) (PPN60).

We note that the Minister for Planning has authorised the preparation and exhibition of Amendment C269yara to introduce a new Municipal Planning Strategy, local policies and supporting documents into the Yarra Planning Scheme. This amendment has gone on exhibition but is at an early stage of the amendment

process, therefore the advice provided in this report has been informed by the relevant existing Local Planning Policy, in particular clauses 22.02, 22.03 and 22.10.

The following Planning Panels Victoria (Panel) reports are relevant to the implementation of the Built Form Review, particularly as many consider the appropriateness of DDOs (containing both mandatory and discretionary provisions) within activity centres (or in the case of Melbourne Amendment C240, the Capital City Zone) that are also subject, in part, to the Heritage Overlay:

- Boroondara C108 'Neighbourhood Centres and Commercial Corridors' (26 February 2014)
- Moreland Planning Scheme Amendment C134 'Brunswick Activity Centre' (15 May 2015)
- Melbourne Planning Scheme Amendment C240 'Bourke Hill' (4 May 2015)
- Bayside Planning Scheme Amendments C113, C114 and C115 'Mandatory provisions for the Sandringham Village, Bay Street and Church Street Activity Centres' (14 January 2015)
- Whitehorse Planning Scheme Amendment C175 'Box Hill Metropolitan Activity Centre' (6 October 2017).
- Stonnington Planning Scheme Amendment C223 'Glenferrie Road and High Street Activity Centre' (15 December 2017)
- Darebin Planning Scheme Amendment C161 'Fairfield Village' (3 December 2018)
- Yarra Planning Scheme Amendment C220 'Johnston Street Built Form Controls' (22 February 2019)
- Melbourne Planning Scheme Amendment C308 'Central Melbourne Urban Design' (16 May 2019)
- Melbourne Planning Scheme Amendment C258 'Heritage Policies Review' (21 May 2019)
- Yarra Planning Scheme Amendment C231 'Queens Parade Built Form Review' (31 October 2019)
- Yarra Planning Scheme Amendment C191 'Swan Street Built Activity Centre' (15 October 2020).

The following reports have also informed this study:

- 'Review & Development of the City of Yarra Landmarks Policy – Landmarks & Views Assessment' prepared by Ethos Urban for the City of Yarra, October 2019.
- 'Fitzroy & Collingwood Built Form Review Stage 2: Victoria Parade Built Form Framework', Hansen Partnership, April 2020.
- Previous heritage built form reports and analysis prepared by GJM Heritage for Brunswick and Smith Street and Queens Parade study areas.

We have approached the preparation of our heritage analysis as follows:

1. Completion of a desktop review of the above listed documents, heritage mapping and grading information, and the Statements of Significance for

heritage places within the study area, including those places included in the Victorian Heritage Register (VHR). The extent of the Heritage Overlays were cross-checked against Google Streetview and VicPlan. This preliminary review familiarised the project team with the heritage fabric of the study area prior to fieldwork being undertaken.

2. Completion of fieldwork by Jim Gardner. All buildings and structures within the study area were inspected from the public realm with particular attention paid to the presentation of heritage buildings to the public realm (principally the street frontage). The rear and side interfaces to the neighbouring residential areas subject to the Heritage Overlay were also considered, where relevant. The purpose of the fieldwork was to:
 - Review the suitability of the extent of the existing Heritage Overlays and to identify if gaps existed.
 - Review the suitability of the existing Statements of Significance for heritage places against the extant heritage fabric and to identify where the statements required updating for the purposes of properly considering built form recommendations.
 - Review the extant heritage fabric against the heritage gradings contained within Appendix 8 and the Yarra Heritage Grading Map to identify any inconsistencies and inaccuracies.
 - Review the heritage buildings and streetscapes within the study area to identify the architectural and streetscape heritage features (e.g. parapets, roof forms, view lines, corner sites) that are relevant to a consideration of built form recommendations.
3. Participation in a workshop with Council and Hansen Partnership. The workshop:
 - Reviewed the proposed ‘built form precincts’ within the study area, characterised by existing built form characteristics.
 - Identified the desired future character of the built form precincts against heritage analysis and state and local planning policy drivers.
 - Identified local landmarks within each streetscape or precinct.
 - Tested built form parameters for new development against the existing heritage fabric utilising cross-sectional drawings with sight-lines taken at natural eye level (1.6m) on the public footpath, and 3D modelling prepared by Hansen Partnership. Views were only considered from public streets; laneway and private realm views were not assessed.
4. Finalisation of heritage recommendations for new built form parameters having considered the above.

2.0 ANALYSIS OF THE PLANNING CONTEXT

2.1 ACTIVITY CENTRE PLANNING AND HERITAGE

The *Planning & Environment Act 1987* and the Victoria Planning Provisions (VPPs) requires planning and responsible authorities to take a balanced approach to strategic and statutory planning functions that consider potentially competing objectives in an integrated manner to deliver a net community benefit for current and future generations.

The objectives of planning in Victoria as set out in Section 4(1) of the Planning and Environment Act are:

- *To provide for the fair, orderly, economic and sustainable use and development of land.*
- *To provide for the protection of natural and man-made resources and the maintenance of ecological processes and genetic diversity.*
- *To secure a pleasant, efficient and safe working, living and recreational environment for all Victorians and visitors to Victoria.*
- *To conserve and enhance those buildings, areas or other places which are of scientific, aesthetic, architectural or historical interest, or otherwise of special cultural value.*
- *To protect public utilities and other assets and enable the orderly provision and coordination of public utilities and other facilities for the benefit of the community.*
- *To facilitate development in accordance with the objectives set out in the points above.*
- *To facilitate the provision of affordable housing in Victoria.*
- *To balance the present and future interests of all Victorians.*

Clause 71.02-3 of the VPP addresses 'integrated decision making', and states:

Society has various needs and expectations such as land for settlement, protection of the environment, economic well-being, various social needs, proper management of resources and infrastructure. Planning aims to meet these by addressing aspects of economic, environmental and social wellbeing affected by land use and development.

Planning and responsible authorities should endeavour to integrate the range of planning policies relevant to the issues to be determined and balance conflicting objectives in favour of net community benefit and sustainable development for the benefit of present and future generations. However, in bushfire affected areas, planning and responsible authorities must prioritise the protection of human life over all other policy considerations.

Planning authorities should identify the potential for regional impacts in their decision making and coordinate strategic planning with their neighbours and other public bodies to achieve sustainable development and effective and efficient use of resources.

Activity Centres that are also subject to Heritage Overlay controls, such as parts of the Victoria Parade Precinct, are an example of where the tension between competing planning objectives must be resolved in a balanced way. The Victoria Parade Precinct has excellent public transport connections, and is in close proximity to vibrant retail, commercial and hospitality centres. The precinct also includes a number of sites of various sizes that can accommodate new, larger scale, development without adversely affecting the heritage values of surrounding heritage places. In order to balance the demand for more intensive development with the management of heritage values embodied in buildings and precincts, it is considered necessary that any DDO – and the background work that underpins it – specifically includes heritage considerations.

Amendment C269 proposes to introduce Clause 11.03-1L to the Yarra Planning Scheme which provides local policy in relation to Major, Neighbourhood and Local Activity Centres and designates the majority of the northern side of Victoria Parade between Nicholson and Wellington streets as a Major Activity Centre (MAC) (Figure 5).



LEGEND

Yarra LGA boundary	Major Activity Centre
Open space	Neighbourhood Activity Centre
Water body	

Figure 5. Major and Neighbourhood Activity Centres in Fitzroy (Council endorsed version of Clause 11.03-1L)



2.2 YARRA PLANNING SCHEME – HERITAGE PROVISIONS

Council has well-established heritage provisions within its planning scheme at Clauses 21.05-1 and 22.02. Also of relevance to the protection of the heritage values of the study area is Clause 22.03, which includes policy to protect the visual prominence of landmarks visible from within the study area and Clause 22-10 which includes policy for new development abutting land within the Heritage Overlay.

2.2.1 Heritage Policy

The relevant objective within Clause 21.05-1 'Heritage' of the Municipal Strategic Statement (MSS) is *Objective 14: To protect and enhance Yarra's heritage places*. The strategies to implement this objective are:

- *Strategy 14.1 - Conserve, protect and enhance identified sites and areas of heritage significance including pre-settlement ecological heritage.*
- *Strategy 14.2 - Support the restoration of heritage places.*
- *Strategy 14.3 - Protect the heritage skyline of heritage precincts.*
- *Strategy 14.4 - Protect the subdivision pattern within heritage places.*
- *Strategy 14.5 - Protect the significant landscape and heritage within streets, parks, gardens, waterways or other open spaces*
- *Strategy 14.6 - Protect buildings, streetscapes and precincts of heritage significance from the visual intrusion of built form both within places and from adjoining areas.*
- *Strategy 14.7 Protect sites of significance to Aboriginal people.*
- *Strategy 14.8 Apply the Development Guidelines for sites subject to a Heritage Overlay policy at clause 22.02.*
- *Strategy 14.9 Apply the Landmarks and Tall Structures policy at clause 22.03.*

Objective 14 and its associated strategies are considered to be generally compatible with appropriately sited and scaled higher density development within the Victoria Parade Precinct where it is subject to the Heritage Overlay. Strategy 14.3 to 'Protect the heritage skyline of heritage precincts' would not be achieved unless new upper-level development was to be of such low scale that it was fully concealed when viewed from the opposite side of the street as defined by the sightline tests described in Figures 2 and 3 of Clause 22.02. Avoiding any new visible built form above existing buildings within the Heritage Overlay - although achieving the 'best' heritage outcome - would not enable a level of development that may reasonably be expected to be achieved within this MAC, nor meet other strategic directions of the Yarra Planning Scheme. A balance therefore needs to be struck between achieving the outcome sought by Strategy 14.3 and meeting the development objectives of the City of Yarra. An acceptable heritage outcome would be one where, although new built fabric is visible above the parapets, roofline or chimneys of these buildings, the development is of a scale, setback and massing such that it retains the primacy of the heritage streetscape and avoids visually dominating the existing buildings.

Clause 22.02 'Development Guidelines for Sites Subject to the Heritage Overlay' provides detailed guidance for development of places within the Heritage Overlay, including demolition. The relevant objectives of Clause 22.02 are:

- *To conserve Yarra's natural and cultural heritage.*
- *To conserve the historic fabric and maintain the integrity of places of cultural heritage significance.*
- *To retain significant view lines to, and vistas of, heritage places.*
- *To preserve the scale and pattern of streetscapes in heritage places.*
- *To encourage the preservation, maintenance, restoration and where appropriate, reconstruction of heritage places.*
- *To ensure the adaptation of heritage places is consistent with the principles of good conservation practice.*
- *To ensure that additions and new works to a heritage place respect the significance of the place.*
- *To encourage the retention of 'individually significant' and 'contributory' heritage places.*
- *To protect archaeological sites of cultural heritage significance.*

Again, these objectives do not preclude higher density development within the Victoria Parade Precinct with the possible exception of '*To preserve the scale ... of streetscapes in heritage places*'.

The demolition policy provided at Clause 22.02-5.1 encourages the retention of 'individually significant' and 'contributory' buildings within a heritage precinct. Removal of part of a heritage place or a contributory element is contemplated if (in general terms) it can be demonstrated that the removal of the part will not adversely affect the significance of the building, or – for a contributory building – the part is not visible from the street, abutting a park or public open space.

With the exception of those heritage places included on the VHR – and therefore regulated under the *Heritage Act 2017* – the significance of the heritage buildings and precincts within the study area lies primarily in fabric visible from the public realm. Therefore, in most circumstances, the heritage controls within the Yarra Planning Scheme effectively limits the control of heritage fabric within the study area to that which is visible from the street, including primary building facades, rear laneway views (where they exist) and visible roof and chimney elements.

In relation to 'New Development, Alterations and Additions', Clause 22.02-5.7.1 sets out the following policy:

General

Encourage the design of new development and alterations and additions to a heritage place or a contributory element to a heritage place to:

- *Respect the pattern, rhythm, orientation to the street, spatial characteristics, fenestration, roof form, materials and heritage character of the surrounding historic streetscape.*

- *Be articulated and massed to correspond with the prevailing building form of the heritage place or contributory elements to the heritage place.*
- *Be visually recessive and not dominate the heritage place.*
- *Be distinguishable from the original historic fabric.*
- *Not remove, cover, damage or change original historic fabric.*
- *Not obscure views of principle façades.*
- *Consider the architectural integrity and context of the heritage place or contributory element.*

Encourage setbacks from the principal street frontage to be similar to those of adjoining contributory buildings; where there are differing adjoining setbacks, the greater setback will apply.

Encourage similar façade heights to the adjoining contributory elements in the street. Where there are differing façade heights, the design should adopt the lesser height.

Minimise the visibility of new additions by:

- *Locating ground level additions and any higher elements towards the rear of the site.*
- *Encouraging ground level additions to contributory buildings to be sited within the ‘envelope’ created by projected sight lines (see Figure 1).*
- *Encouraging upper level additions to heritage places to be sited within the ‘envelope’ created by projected sight lines (for Contributory buildings refer to Figure 2 and for Individually significant buildings refer to Figure 3).*
- *Encouraging additions to individually significant places to, as far as possible, be concealed by existing heritage fabric when viewed from the front street and to read as secondary elements when viewed from any other adjoining street.*

Discourage elements which detract from the heritage fabric or are not contemporary with the era of the building such as unroofed or open upper level decks or balconies, reflective glass, glass balustrades and pedestrian entrance canopies.

The policy for full or partial concealment of rear additions to residential buildings as described in Figures 2 and 3 of the General Policy at Clause 22.02 is modified by the Specific Requirements at Clause 22.02-5.7.2 that applies to corner sites and sites with dual frontages, and industrial, commercial and retail heritage places:

Corner Sites and Sites with Dual Frontages

Encourage new building and additions on a site with frontages to two streets, being either a corner site or a site with dual street frontages, to respect the built form and character of the heritage place and adjoining or adjacent contributory elements to the heritage place.

Encourage new buildings on corner sites to reflect the setbacks of buildings that occupy other corners of the intersection.

...

Industrial, Commercial and Retail Heritage Place or Contributory Elements

Encourage new upper level additions and works to:

- *Respect the scale and form of the existing heritage place or contributory elements to the heritage place by being set back from the lower built form elements. Each higher element should be set further back from lower heritage built forms.*
- *Incorporate treatments which make them less apparent.*

The specific provisions prevail over the general policy where there is a conflict or inconsistency. This establishes an expectation that new development behind industrial, commercial and retail buildings within the Heritage Overlay is not going to be fully or substantially concealed from public realm views. In addition, it should be noted that Victoria Parade, being a boulevard with multi-lane carriageways and a broad median strip, is 60m wide (building line to building line) where the residential examples shown in Figures 2 and 3 of Clause 22.02-5.7.1 show a sightline test from across an approx. 10m wide residential street. In effect, the wider the street, the more visible a new rear development will be.

Although a greater level of concealment would generally provide a better heritage outcome, this specific sightline-based guidance in the heritage policy is designed to preserve and enhance the character and appearance of predominantly one and two-storey dwellings within more typical narrow residential streets and is not readily applied to the wider form of Victoria Parade.

It is also considered that the policy at 22.02-5.7.1 to 'Discourage elements which ... are not contemporary with the era of the building such as ... reflective glass, glass balustrades and pedestrian entrance canopies' may not achieve an appropriate urban design and architectural outcome within a commercial and industrial setting such as that present at the Alexandra Parade Precinct. In such areas, a 'contrasting' or 'interpretative' design approach for new taller development above the heritage building is likely to be more recessive than a 'respectful' or 'historicist' one that would lead to the new additions inappropriately mimicking the historic form and potentially being more visually intrusive.

2.2.2 Landmarks and Tall Structures

Clause 22.03 – 'Landmarks and Tall Structures' identifies a number of landmark buildings and advertising signs to which views should be protected. None of the landmarks and tall structures are located within the study area, nor does the study area provide any 'primary' or 'secondary' views of these structures as defined by the *Landmarks & Views Assessment* (Ethos Urban, October 2019).

2.2.3 Heritage Overlay

The head heritage provision of the VPP, Clause 43.01 'Heritage Overlay', has the following purpose:

- *To implement the Municipal Planning Strategy and the Planning Policy Framework.*
- *To conserve and enhance heritage places of natural or cultural significance.*
- *To conserve and enhance those elements which contribute to the significance of heritage places.*
- *To ensure that development does not adversely affect the significance of heritage places.*
- *To conserve specifically identified heritage places by allowing a use that would otherwise be prohibited if this will demonstrably assist with the conservation of the significance of the heritage place.*

Clause 43.01-8 sets out 'Decision Guidelines' – in addition to those included in Clause 65 – that the Responsible Authority must consider before determining a permit application. These are:

- *The Municipal Planning Strategy and the Planning Policy Framework.*
- *The significance of the heritage place and whether the proposal will adversely affect the natural or cultural significance of the place.*
- *Any applicable statement of significance (whether or not specified in the schedule to this overlay), heritage study and any applicable conservation policy.*
- *Any applicable heritage design guideline specified in the schedule to this overlay*
- *Whether the location, bulk, form or appearance of the proposed building will adversely affect the significance of the heritage place.*
- *Whether the location, bulk, form and appearance of the proposed building is in keeping with the character and appearance of adjacent buildings and the heritage place.*
- *Whether the demolition, removal or external alteration will adversely affect the significance of the heritage place.*
- *Whether the proposed works will adversely affect the significance, character or appearance of the heritage place.*
- *Whether the proposed subdivision will adversely affect the significance of the heritage place.*
- *Whether the proposed subdivision may result in development which will adversely affect the significance, character or appearance of the heritage place.*
- *Whether the proposed sign will adversely affect the significance, character or appearance of the heritage place.*
- *Whether the lopping or development will adversely affect the health, appearance or significance of the tree.*

- *Whether the location, style, size, colour and materials of the proposed solar energy facility will adversely affect the significance, character or appearance of the heritage place.*

While some of these considerations are not obviously consistent with the addition of higher density development behind heritage buildings, the first purpose of 43.01 and the first decision guideline encompasses the whole Municipal Planning Strategy and the Planning Policy Framework (integrated decision-making). Therefore, a balance must be struck by the Responsible Authority between achieving the objectives of the Heritage Overlay and meeting the objectives of other parts of the VPPs including Activity Centre policy and commercial zoning. There is established precedent for new rear development to be accommodated behind heritage buildings in commercial precincts throughout inner Melbourne without substantially compromising the identified heritage values of these heritage places.

3.0 HERITAGE IN DESIGN AND DEVELOPMENT OVERLAYS – PANEL FINDINGS

Planning Panels Victoria has considered a number of Planning Scheme Amendments that are of particular relevance to this project: Bayside Amendments C113, C114 and C115, Boroondara C108, Darebin Amendment C161, Moreland Amendment C134, Melbourne Amendments C240, Stonnington Amendment C223, Whitehorse Amendment C175 and Yarra Amendments C220, C231 and C191.

Panels for these Amendments considered the appropriateness of mandatory controls in the context of PPN59 and, in their recommendations, provided guidance on which circumstances mandatory controls should be applied. In response to submissions, they also considered the issue of whether or not the DDO control should include objectives to protect heritage or whether this should be the sole domain of the Heritage Overlay provisions. In addition to these panel reports, Amendment C123 to the Banyule Planning Scheme, approved via ministerial intervention, provides further instruction as to the role of mandatory controls.

These reports also provide useful guidance on the form and wording of DDO controls.

In summary, Panel has concluded that:

- The Heritage Overlay identifies what is significant within an Activity Centre.
- Heritage is an appropriate issue for DDOs to provide guidance on to inform future development.
- Mandatory controls should be used only in exceptional circumstances and their application should be guided by PPN59 and PPN60.
- Formulae defining the proportion of new built form that can be viewed above the street wall is an appropriate mechanism for informing the design and massing of new built form.

In this project, the approach taken in the formulation of the built form controls to manage development affecting heritage places is to complement existing policy. Clause 22.02 - 'Development Guidelines for Sites Subject to the Heritage Overlay' and relevant parts of Clause 22.10 – 'Built Form and Design Policy' have been taken as the starting point for the development of these complementary controls and policy.

Where existing policy is considered to be satisfactory, no additional policy has been recommended. However, specific policy has been recommended where it is considered necessary to provide guidance to recognise the current role of the Victoria Parade Precinct as a major boulevard and to enable its future development while protecting their heritage values.

A discussion of the most relevant of the Panel reports is provided below, and at Section 3.9 the recommendations for each panel are summarised with comment on the implications of the outcome.

3.1 YARRA PLANNING SCHEME AMENDMENT C231

GJM Heritage prepared the Queens Parade Built Form Heritage Analysis and Recommendations (11 December 2017) which informed C231yara. C231yara seeks to apply built form controls in the form of DDOs to Queens Parade, Fitzroy North and Clifton Hill and amend Heritage Overlay controls that apply within the study area. Precinct 4 within DDO20 covers the commercial shopping strip that forms part of the Queens Parade Neighbourhood Activity Centre. Like Queens Parade, Victoria Parade as a 60m wide boulevard albeit without the same degree of consistency of built form found within that commercial High Street.

Of relevance to the Alexandra Parade Precinct, the Panel for Amendment C231 found that the strategic work undertaken in support of the Amendment was strong and that it assisted in justifying the majority of the built form parameters recommended in the DDOs, particularly with respect to mandatory controls. At p29 of the Panel Report, the Panel notes that:

Exceptional circumstances exist for the application of mandatory controls for development as the QPAC (Queens Parade Activity Centre) includes a number of significant and contributory heritage places and heritage fabric set within a consistent streetscape form.

The Panel recognised that the wider, boulevard context would lead to a high visibility of upper-level development, which in turn warranted the application of height limits, and linked the use of mandatory (instead of preferred) controls to the consistency of the heritage streetscape.

The Panel supported the mandatory upper-level setback of 8m within the Council preferred DDO and the combination of mandatory and preferred height controls where this provided certainty where distinctive heritage fabric warranted greater protection. It also recognised that an Activity Centre with diverse built form can have areas of little change where growth can be accommodated elsewhere within the Centre. Further, the Panel agreed that it was appropriate for the proposed mandatory built form controls within DDO16 to protect the key views of local landmarks and those identified in Clause 22.03.

3.2 YARRA PLANNING SCHEME AMENDMENT C220

Johnston Street in Collingwood and the western part of Abbotsford (west of the railway viaduct) is a highly intact, predominantly Victorian/early Edwardian-era streetscape covered by the Heritage Overlay. Those parts of Victoria Parade included within the Heritage Overlay have similarly high proportion of 'Contributory' and 'Individually Significant' buildings with a high level of integrity, and similar existing street wall heights (generally between 8m and 11m) as Johnston Street.

C220yara introduced built form controls along Johnston Street in the form of DDO15. The Panel report recommended the inclusion of the following DDO objective which is also relevant to those parts of Victoria Parade subject to the Heritage Overlay:



To preserve the valued heritage character of the streetscape and ensure that the predominantly two storey (heritage scale) street-wall remains the visually prominent built form of Johnston Street west of the railway line bridge...

The Panel report provides commentary which is of relevance to a consideration of the proposed built form controls for Victoria Parade. In particular, the Panel stated:

In urban design terms, the 6 metre setback will retain the 'human scale' of Johnston Street, secure the distinction between the street wall and upper levels and will reduce the potential for overshadowing and adverse wind conditions.

...

The Panel does not agree that less significant sections [of Johnston Street] warrant a different treatment. Less significant areas equally deserve to exhibit the overall urban design outcome: a strong street wall with a distinct setback to the mid level form.

To achieve these objectives Panel recommended that a building envelope requirement be established that, rather than being based on a sightline test from the opposite side of the street new, required new development to be within a 45° 'angular plane' drawn from the maximum street wall height. In combination with upper-level front setbacks and maximum building heights the angular plane creates a further upper-level setback consistent with the application of the policy objective at Clause 22.02-5.7.1 that each higher element to industrial, commercial and retail buildings should be set further back from the lower heritage built form.

Figure 6: Building envelope requirement – Heritage Building (Figure 1 in Schedule 15 to Clause 43.02 Design and Development Overlay).

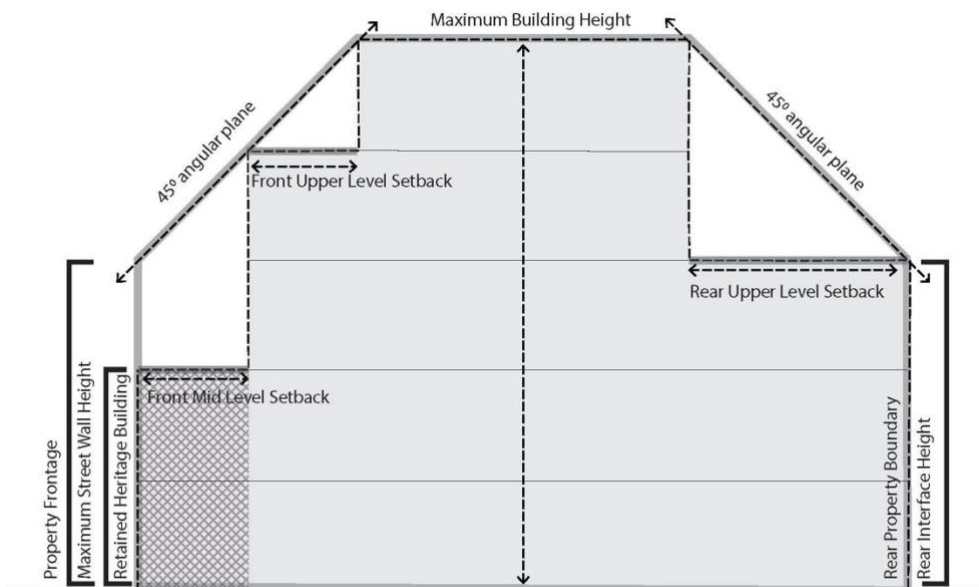
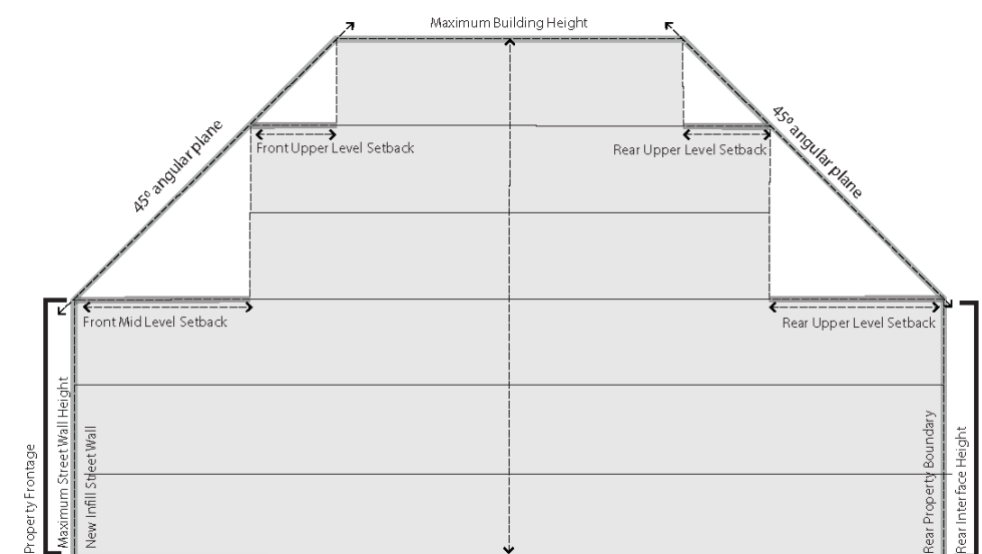


Figure 7: Building envelope requirement – Infill Building (Figure 2 in Schedule 15 to Clause 43.02 Design and Development Overlay).



3.3 YARRA AMENDMENT C191

Swan Street, Richmond is a Major Activity Centre with a highly intact turn of the century ‘High Street’ occupying a large proportion of its length, as well as smaller precincts and individual heritage places dispersed along its full extent.

Amendment C191yara proposes to introduce four DDOs (DDO25, DDO26, DDO27 and DDO28) to the Activity Centre, with the different controls reflecting the different existing physical conditions and the potential development opportunities evident throughout the Activity Centre.

In its report of 15 October 2020, the Panel supported the use of mandatory controls for street wall and 6m upper-level setbacks for individually significant heritage places and intact heritage streetscapes, as well as mandatory controls for overall building heights in intact heritage streetscapes. Mandatory controls were also supported to protect views to local landmarks.

For parts of the Activity Centre that present a less consistent and more diverse built form expression, discretionary controls were considered to be appropriate.

In contrast to the Panel considering C220yara, the C191yara Panel considered that it was unnecessary to provide additional parameters to guide the form of upper level development, instead finding that the combination of specified heights, setbacks and design requirements for new upper-level development to be “visually recessive”, were sufficient.

3.4 MORELAND AMENDMENT C134

Sydney Road, Brunswick is a Major Activity Centre with a highly intact, predominantly Victorian streetscapes that is subject to the Heritage Overlay. Gazetted on 11 August 2016, C134more introduced DDO18, DDO19 and DDO20. DDO18 set mandatory street wall heights on Sydney Road north of Brunswick Road of between 8m and 11m.



DDO18 provides a preferred minimum 5m setback for development above the street wall and to establish a preferred ratio of $\frac{3}{4}$: $\frac{1}{4}$ street wall to new built form through the following design objective:

- *Be designed to ensure that it occupies no more than one quarter of the vertical angle defined by the whole building in the view from an eye-level of 1.7 metres on the opposite side of the street, as illustrated in Figure 1 below.*

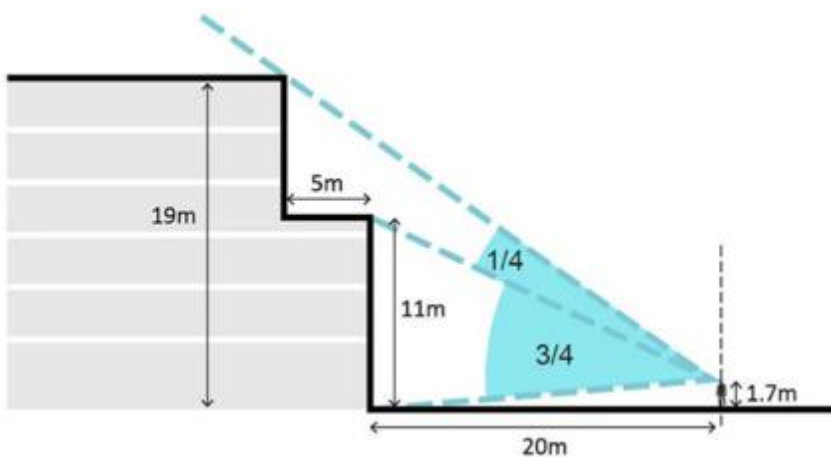


Figure 8: Upper-level setbacks along Sydney Road (Figure 1 in Moreland DDO18).

DDO18 also provides a useful model for dealing with upper-level development where an existing heritage building in the heritage streetscape has a street wall height of less than the 11m street wall height provided in that control:

- *Where an existing building with a street wall height of less than 11 metres is to be retained for heritage reasons new development may occupy more than one quarter of the vertical angle defined by the whole building outlined in Figure 1 [Figure 8 of this report] above.*

3.5 BOROONDARA AMENDMENT C108

The Panel considering C108boro discussed the use of mandatory street wall height, upper-level setbacks and overall heights across 31 Neighbourhood Activity Centres and three commercial corridors (Camberwell Road/Burwood Road and Canterbury Road).

In its report dated 26 February 2014, the Panel noted its strong support for the protection of heritage assets in Boroondara and recommended reinstatement of policy in the exhibited Amendment that encouraged new development on or adjoining a heritage place to be moderated. In particular, the Panel recommended that policy guidance be included that:

The combination of the height, setbacks and design treatment of new buildings should ensure a heritage place on or adjoining the site is not overwhelmed or dominated.

The Panel also considered the use of mandatory height and setback controls, and recognised that the version of *Plan Melbourne* at that time foreshadowed stronger policy support for the use of mandatory provisions in neighbourhood centres (and residential areas) to increase planning certainty.

The Panel report recognised that mandatory provisions that prescribed standards without a capacity for departures have been supported in areas of consistently high heritage value with consistent character. While acknowledging the heritage values and ‘main street’ character of the Neighbourhood Activity Centres subject to C108, the Panel also recognised that new development will be visible behind the retained façades – particularly from oblique views – and that invisibility of upper-level development is either unreasonable or not necessary to maintain the primacy of the street wall.

In conclusion, the Panel accepted some use of mandatory controls within Boroondara’s neighbourhood centres, but not in the commercial corridors:

The Panel recognises that Plan Melbourne foreshadows stronger policy support for the use of mandatory provisions in neighbourhood centres (and residential areas) to increase certainty. The Panel considers the combination of the street wall and upper level setbacks is critical in neighbourhood centres to maintain the established main street character and in these situations mandatory controls can be justified. However, we consider development with elements that exceed the nominated height and/or adopt alternative setbacks should not be precluded as they may produce better outcomes in some circumstances. The overall maximum height limits should therefore remain discretionary to allow for such circumstances.

It was the Panel’s conclusion that mandatory street wall heights which reflected the dominant character of the neighbourhood centres were acceptable (either 8m or 11m, depending on the context). It also found that if mandatory upper-level setbacks were to be adopted, they should be sufficient to ensure that in most cases the upper-storey will be clearly distinguishable from the street wall of the heritage building and be a recessive element in neighbourhood centres streetscapes. To achieve this, the Panel identified 5m as being an appropriate mandatory minimum setback for upper-level development in the context of Boroondara’s Neighbourhood Activity Centres.

3.6 WHITEHORSE AMENDMENT C175

C175whit sought to implement the *Box Hill Metropolitan Activity Centre Built Form Guidelines* (Hansen Partnership, 2016) by rezoning land, introducing the Built Form Guidelines as a reference document and applying a new DDO Schedule to introduce built form controls. In its consideration of this Amendment, the Panel Report dated 6 October 2017 stated:

The Panel would have benefited from a more sophisticated analysis of the heritage precinct that utilised three-dimensional modelling, sight lines and view-sheds to help understand the rationale for the proposed heritage related controls. Without this basic information, it is difficult to determine whether the proposed controls are appropriate...

and concluded that in the absence of this modelling:

- *The Built Form Responses regarding Heritage should not proceed in their current form.*

The absence of 3D modelling, and sight line and view-shed analysis in relation to those areas of the Box Hill Activity Centre that are subject to the Heritage Overlay appears to have been critical in Panel recommending that the proposed built form controls not be applied to address heritage.

3.7 STONNINGTON AMENDMENT C223

The Glenferrie Road and High Street Major Activity Centre encompasses the two linear commercial strips of Glenferrie Road and High Street in Malvern as well as two peripheral areas. The Heritage Overlay, which covers all of Glenferrie Road and most of High Street, acknowledges the area for its *'metropolitan significance as one of the major strip shopping centres to have retained its role into the late twentieth century, and for the quality and integrity of its Victorian, Federation and Interwar building stock'*.¹ C223ston sought to apply new built form provisions through the application of DDO19 to the entire Activity Centre, with precincts A and B covering the commercial and heritage precincts of Glenferrie Road and High Street respectively.

While the Amendment proposed an 8-10m setback above the street wall for precincts A and B, the Panel found it to be effectively a concealment of upper-level additions, supporting instead a 5m setback as adequate to respect heritage values without removing development capacity. This was derived from the precedent in the Boroondara Planning Scheme and was seen to equate to the typical first room of a Victorian-era building. The Amendment was otherwise generally supported by the Panel as an appropriate balance between protecting heritage values and enabling growth. Discretionary preferred maximum building heights between 14.5 metres (4 storeys) and 21 metres (6 storeys) were supported through precincts A and B.

The Panel also reviewed the drafting of discretionary and mandatory provisions, addressing the appropriateness of the terms 'should' and 'must'. The Panel noted that confusion arose from the DDO parent clause, and until such time as the clause is redrafted, the term 'must' is to be used for schedule requirements with the addition of further clarification if it can be varied with a permit.

3.8 DAREBIN AMENDMENT C161

C161dare proposed to implement the *'Fairfield Village Heritage Assessment, 2017'* (Heritage Intelligence) and *'Fairfield Village Built Form Guidelines 2017'* (Hansen Partnership) through the application of Heritage Overlay (HO313) and DDO21 to the Fairfield Village Neighbourhood Centre. DDO21 created two sub precincts: Area 1 to

¹ Retrieved from Victorian Heritage Database, 18 January 2018 (<https://vhd.heritagecouncil.vic.gov.au/places/31530>)

be applied to the proposed HO313 precinct; with the remainder of the Neighbourhood Centre covered by Area 2.

The Panel found the application of the Heritage Overlay in conjunction with the DDO would enable the precinct ‘to support a variety of housing typologies at increased densities’ in a way that ‘allows the heritage place to be identified and understood’. Further, the Panel supported the application of a mixture of mandatory and discretionary controls to Area 1 of the DDO in the form of:

- Mandatory maximum building heights at 14.5m and 17.5m (four and five storey), triggered by a lot width of 24m for five-storey.
- Mandatory maximum street wall height to be the greater of 8.5m or the adjacent street wall.
- Discretionary minimum front setbacks above the street wall at generally 4m, and 8m if constructing to a fifth level.
- The addition of a 3m side setback at the fifth-floor level – introduced as a discretionary provision to prevent the creation of a dominating wall of development along Station Street.

3.9 SUMMARY

Table 1 – Summarised recommendations and implications

YARRA AMENDMENT C231	
<i>Recommendation</i>	<i>Implications</i>
Significant and contributory heritage places and heritage fabric set within a consistent streetscape form.	Gertrude Street displays similar levels of consistency in heritage streetscape to warrant mandatory controls.
8m setbacks.	An 8m mandatory setback can be justified for highly intact heritage streetscape.
Combination of preferred and mandatory heights.	The use of a balanced combination of preferred and mandatory heights is appropriate to respond to varied conditions.
Limiting heights within heritage precincts while allowing housing capacity to be met elsewhere in the broader precinct.	The most highly intact areas warrant low heights to protect heritage streetscapes.
YARRA AMENDMENT C220	
<i>Recommendation</i>	<i>Implications</i>
A 6m upper-level setback will retain the ‘human scale’ of Johnston Street, secure the distinction between the [heritage] street wall and upper-levels.	A 6m mandatory upper-level setback is an appropriate minimum.
The less significant sections of Johnston Street do not warrant lesser built form controls.	The same controls should be applied within the DDO irrespective of the significance of the street.



<p>A preferred ratio of 2/3:1/3 street wall to new upper-level built from should be replaced with a 45 degree angular plane.</p>	<p>A 45 degree angular plane above a nominal 11m street wall height can inform the preferred mid-level built form rather than a ratio based sightline test.</p>
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<p>In combination with upper-level front setbacks and maximum building heights the angular plane creates a further upper-level setback from the mid-level setback.</p>	<p>Upper-level development should be set further back from the street wall consistent with the guidance at 22.02-5.7.2.</p>
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YARRA AMENDMENT C191

<i>Recommendation</i>	<i>Implications</i>
Combination of preferred and mandatory heights.	The use of a balanced combination of preferred and mandatory heights is appropriate to respond to varied conditions.
Limiting heights within heritage precincts while allowing housing capacity to be met elsewhere in the broader precinct.	The most highly intact areas warrant low heights to protect heritage place. Larger scale development should be encouraged outside these heritage places.
A 6m upper-level setback is necessary to avoid facadism and to retain the prominence of the heritage street wall	A 6m mandatory upper-level setback is an appropriate minimum for site-specific Heritage Overlays and intact heritage streetscapes.
The combination of upper-level front setbacks, maximum building heights and design requirements in respect of upper-level development is sufficient to manage taller built form in heritage contexts.	Further guidance in the form of a sight-line test or angular plane formulae is not warranted. Note: this conclusion differs from that of the Panel that considered C220yara.

MORELAND AMENDMENT C134

<i>Recommendation</i>	<i>Implications</i>
The application of mandatory street wall heights to Sydney Road is justified.	Provides a justification for the application of mandatory street wall heights within the Study Area.
Established a preferred ratio of ¾ : ¼ street wall to new upper-level built form.	The use of a sightline test to inform new upper-level built form is appropriate.

BOROONDARA AMENDMENT C108

<i>Recommendation</i>	<i>Implications</i>
The combination of the height, setbacks and design treatment of new buildings should ensure a heritage place on or adjoining the site is not overwhelmed or dominated.	The DDO can included height, setback and design treatment controls to avoid new development dominating heritage places.
New development will be visible behind the retained façades – particularly from oblique views – and that invisibility of	Some visibility of new upper-level development (including from oblique



upper-level development is either unreasonable or not necessary to achieve the primacy of the street wall.

views) will be acceptable and complete concealment is not necessary.

Mandatory upper-level setbacks to the commercial corridors are justified.

Provides a justification for the application of mandatory upper-level setbacks within the study area.

WHITEHORSE AMENDMENT C175

Recommendation

Implications

In the absence of modelling, built form heritage controls should not proceed.

That 3D modelling, sightlines and viewshed analysis should inform built form controls.

STONNINGTON AMENDMENT C223

Recommendation

Implications

Panel supported a 5m upper-level setback instead of the 8-10m setbacks proposed that effectively concealed upper-level development.

There is an expectation that the visibility of some new upper-level built form will be acceptable and complete concealment is not necessary.

Application of the words 'should' and 'must' within controls.

Use 'should' used for preferred controls and 'must' for mandatory controls.

DAREBIN AMENDMENT C161

Recommendation

Implications

The application of mandatory building heights to Fairfield Village is justified.

Provides a justification for the application of mandatory building heights within the Study Area.

The application of mandatory street wall heights to Fairfield Village is justified.

Provides a justification for the application of mandatory street wall heights within the Study Area.



4.0 MANDATORY AND DISCRETIONARY HEIGHT AND SETBACK CONTROLS

Planning Practice Note 59: The Role of Mandatory Provisions in Planning Schemes (September 2018) (PPN59) notes that the VPPs are predominantly performance-based and that mandatory provisions are the exception. The PPN sets out a series of five criteria against which to test proposed mandatory provisions, being:

- Is the mandatory provision strategically supported?
- Is the mandatory provision appropriate to the majority of proposals?
- Does the mandatory provision provide for the preferred outcome?
- Will the majority of proposals not in accordance with the mandatory provision be clearly unacceptable?
- Will the mandatory provision reduce administrative costs?

Planning Practice Note 60: Height and Setback Controls for Activity Centres (PPN60) provides specific guidance on the use of mandatory height and setback controls in Activity Centres. In September 2018, DELWP published an updated version of PPN60 following the completion of the pilot project *Better Height Controls in Activity Centres*².

Of relevance to this matter, PPN60 provides an additional justification for the use of mandatory controls based on 'comprehensive strategic work', which reads:

Mandatory height or setback controls should only be applied where:

- *exceptional circumstances exist; or*
- *council has undertaken comprehensive strategic work and is able to demonstrate that mandatory controls are appropriate in the context, and*
- *they are absolutely necessary to achieve the preferred built form outcomes and it can be demonstrated that exceeding these development parameters would result in unacceptable built form outcomes.*

In relation to 'exceptional circumstances', PPN60 states:

Exceptional circumstances may be identified for individual locations or specific and confined precincts, and might include:

- *significant heritage places where other controls are demonstrated to be inadequate to protect unique heritage values.*

² Refer to the Panel Report to Yarra C220 chapter 1.2 for further discussion on the pilot project and the amendment to PPN60.

- *sites of recognised State significance where building heights can be shown to add to the significance of the place, for example views to the Shrine of Remembrance...*

To pursue mandatory controls, PPN60 also states:

Where exceptional circumstances are identified, mandatory height and setback controls should only be applied where they are absolutely necessary to achieve the built form objectives or outcomes identified from the comprehensive built form analysis. Where mandatory controls are proposed, it will need to be demonstrated that discretionary controls could result in an unacceptable built form outcome.

The amended version of PPN60 reflects a broader shift over time within the application of the VPPs in favour of the use of mandatory controls.

For this project, the purpose of the Hansen Built Form Review and this report is to provide a comprehensive strategic basis for height and setback controls within the study area.

PPN60 identifies the following criteria for 'exceptional circumstances' that *"...may be identified for individual locations or specific and confined precincts"*. These include (as relevant):

- *significant heritage places where other controls are demonstrated to be inadequate to protect unique heritage values*
- *sites of recognised State significance where building heights can be shown to add to the significance of the place, for example views to the Shrine of Remembrance...*

To pursue mandatory controls, PPN60 also states:

Where exceptional circumstances are identified, mandatory height and setback controls should only be applied where they are absolutely necessary to achieve the built form objectives or outcomes identified from the comprehensive built form analysis. Where mandatory controls are proposed, it will need to be demonstrated that discretionary controls could result in an unacceptable built form outcome.

The Panels that considered C108boro, C161dare, C134, C220yara, C191yara and C231yara provide further guidance on the application of mandatory built form controls along Victoria Parade.

These Panels concluded that for Heritage Overlays within Activity Centres:

- Mandatory controls were appropriate for street wall heights along Sydney Road, in 31 neighbourhood centres in Boroondara and Area 1 of the Fairfield Village Neighbourhood Centre
- Mandatory upper level setbacks were appropriate in many of Boroondara's neighbourhood centres

- Mandatory heights were appropriate for Area 1 of the Fairfield Village Neighbourhood Centre
- Mandatory setbacks were appropriate for Johnston Street with a mixture of preferred and mandatory height limited combined with a 45 degree angular plane test.
- Mandatory height and upper level setback controls were appropriate to protect the most highly consistent and intact parts of Queens Parade and to protect views to key landmarks.
- Mandatory height and upper level setback controls were appropriate to protect the most highly consistent and intact parts of Queens Parade.

Parts of the Victoria Parade Precinct have - albeit short- lengths of highly consistent, intact or cohesive streetscapes that warrant mandatory controls. Mandatory height limits are appropriate to protect the visual primacy of the heritage streetscapes in these locations. Mandatory minimum upper level setback controls are also warranted where it is necessary to protect the legibility and heritage fabric of buildings that are subject to the Heritage Overlay

PART II: HERITAGE ANALYSIS



5.0 STUDY AREA ANALYSIS

5.1 PRECINCT BOUNDARY



Figure 9: Victoria Parade Study Area (black) overlaid on an aerial image (Source: adapted from VicPlan).

As noted previously, this report focuses on the Victoria Parade Precinct as shown in Figure 9 above. The following precincts were considered in the *Brunswick and Smith Street Built Form Review*, GJM Heritage, 25 November 2019:

- Brunswick Street Activity Centre Spine
- Town Hall Mixed Use Precinct
- Smith Street Activity Centre Spine
- Johnston Street Activity Centre Spine
- Fitzroy East Mixed Use Precinct.

Gertrude Street was the subject of the *Gertrude Street Built Form Framework: Heritage Analysis & Recommendations*, GJM Heritage, 9 December 2019.

The Collingwood Mixed Use Precinct was considered as part of a separate study: *Collingwood Mixed Use Pocket Heritage Analysis & Recommendations*, GJM Heritage, 6 June 2018.

The Fitzroy West Mixed-Use and the Alexandra Parade Boulevard precincts are subject of separate reports prepared as part of this study.

The following sections contain an analysis of the heritage components and qualities of the Victoria Parade Precinct, including significant views. An analysis of future built form character considerations has also been provided, along with recommended built form parameters to appropriately manage heritage values.

5.2 HERITAGE CHARACTERISTICS

The buildings within the Heritage Overlay at the western end of the Victoria Parade Precinct are typically two-storey terraced houses dating from the late nineteenth century. These include the State heritage-listed Blanche Terrace (169-179 Victoria Parade, Fitzroy; VHR H0177-H0182) and McClelland Terrace (203 Victoria Parade, Fitzroy; VHR H0590). The heritage buildings in the study area within HO334 – South

Fitzroy Precinct are interspersed with non-contributory post-war buildings including a service station, offices and the Victoria Peak Townhouses at 187-201 Victoria Street, Fitzroy.



Figure 10: (left) South side of Victoria Parade looking east from the intersection with Napier Street.



Figure 14: (right) Terraced housing, 139-143 Victoria Parade, Fitzroy ('Contributory' within HO334)



Figure 15: (left) Alexandra Terrace, 145-149 Victoria Parade, Fitzroy ('Individually significant' within HO334)



Figure 16: (right) Pair of terraced houses, 163-165 Victoria Parade, ('Individually significant' within HO334) and terraced house, 167 Victoria Parade, Fitzroy ('Contributory' within HO334)



Figure 17: Blanche Terrace, 169-179 Victoria Parade, Fitzroy (VHR H0177-H0182).



Figure 18: (left) Russell House, 181 Victoria Parade ('Individually significant' within HO334) and c.1970s offices, 183 Victoria Parade, Fitzroy ('Not-contributory' within HO334)



Figure 19: (right) Victoria Peak Townhouses, 187-201 Victoria Parade, Fitzroy ('Not-contributory' within HO334)



Figure 20: (left) McClelland Terrace, 203 Victoria Parade, Fitzroy (VHR H0590)



Figure 21: (right) Intersection of Victoria Parade and Smith Street looking northwest.

The terraced houses and shop / residences towards the centre of the study area (between Smith and Wellington streets) are included within the Heritage Overlay (HO336 – Victoria Parade Precinct, Collingwood). Between Wellington and Islington street, the study area is typified by large post-war offices and showrooms without any identified heritage value.

Common to other major streets in the City of Yarra, the precinct includes a number of corner hotels: the Baden Powell Hotel (61-65 Victoria Parade, Collingwood; within HO336), the former John Franklin Hotel (75-77 Victoria Parade, Collingwood; within HO336) and the Prince Patrick Hotel (135-141 Victoria Parade, Collingwood; HO138).

As well as common terraced-form residential and commercial building types, the study area includes two single storey Edwardian-era houses (71 & 73 Victoria Parade, Collingwood; within HO336), the Inter-war Melbourne After Care Home (45-47 Victoria Parade, Collingwood; within HO336) and the Former Ebenezer Baptist Church (159 Victoria Parade, Collingwood; HO139).



Figure 22: (left) Intersection of Victoria Parade and Smith Street looking northeast



Figure 23: (right) 'Portia', 15-17 Victoria Parade; Irwell Terrace 19-23 Victoria Parade, ('Individually significant' within HO336); single storey terraced house, 25 Victoria Parade, Collingwood ('Contributory' within HO336)



Figure 24: Shop / residences, 27-37 Victoria Parade, Collingwood ('Contributory' within HO336)



Figure 25: (right) 'Floraston', 39 Victoria Parade; Melbourne After Care Home, 45-47 Victoria Parade, Collingwood ('Individually significant' within HO336)



Figure 26: (left) Shop/residences, 49-59 Victoria Parade, Collingwood ('Contributory' within HO336)



Figure 27: (right) Baden Powell Hotel, 61-63 Victoria Parade; terraced houses 65-67 Victoria Parade; shop/residence, 69 Victoria Parade, Collingwood ('Contributory' within HO336)



Figure 28: (left) Houses, 71-73 Victoria Parade ('Contributory' within HO336); former John Franklin Hotel, 75-77 Victoria Parade, Collingwood ('Individually significant' within HO336)



Figure 29: (right) Large showrooms, west of Rokeby Street





Figure 30: (left) Prince Patrick Hotel and shops, 135-141 Victoria Parade, Collingwood (HO138)



Figure 31: (right) Former Ebenezer Baptist Church, 139 Victoria Parade, Collingwood (HO139)

At the eastern end of the study area, between Islington Street and Hoddle Street, there is an intact row of Victorian-era single and two-storey shop residences (205-219 Victoria Parade, Collingwood) that are not included within the Heritage Overlay, but are recommended for inclusion following a heritage assessment undertaken by GJM in August 2020.



Figure 32: Shop/residences, 205-219 Victoria Parade, Collingwood (not included within an HO).

The heritage buildings found throughout the study area generally share the following characteristics:

- Attached terraced construction
- Masonry construction with less than 40% of the upper-level street wall face comprised with openings such as windows and doors (see Figure 33)
- Painted render or face brick façades
- Parapeted front facades with solid parapets, open balustrades or more elaborate gables
- No setback from the street boundary
- Early or altered shop fronts taking up the majority of the ground floor often with recessed entries
- Splayed corners to return façades or end-of-terrace elevations to laneways and side streets
- Architectural features such as belvederes and towers at prominent corner site
- Verandahs or later canopies



- Visible chimneys normally set back between 3m and 4m from the front of the building.

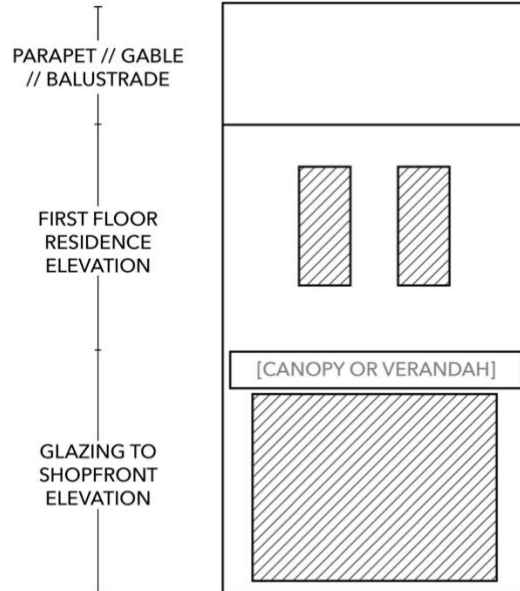


Figure 33: Two-storey shop residence (27 Victoria Parade, Collingwood, within HO336) showing the typical relationship between solid (walls) and void (windows) on the front (street) façade. The first-floor windows make up less than 40% of the wall area (excluding the parapet, pediment or balustrade). The early (or more recent – as in this case) shopfront glazing occupies the majority of the ground floor façade.

The following examples show typical building types found within the study area. It must be noted that buildings of a particular type will not necessarily demonstrate all the features identified below, and may include other features such as visible roofs and chimneys.



Figure 34: Two-storey terraced house ('Floraston', 39 Victoria Parade, Collingwood within HO336).



Figure 35: Shop / Residence (27 Victoria Parade, Collingwood, within HO336)




Figure 36: Corner hotel / commercial building (Prince Patrick Hotel, corner Victoria Parade and Rokeby Street, HO138)

5.3 LOCAL LANDMARKS




Victoria Parade Precinct does not include any of the municipal-wide landmarks within the City of Yarra that are identified within Clause 22.03 ‘Landmarks and Tall Structures’ of the Yarra Planning Scheme nor any of the views of these landmarks included in the report entitled *Landmarks & Views Assessment* (Ethos Urban, October 2019). Having said that, the location, scale, function and architectural form and detail of some buildings within the study area has resulted in them acting as local landmarks. These buildings serve as markers, wayfinding aids or landmarks in the local streetscape context due to their siting at key intersections or their scale within the surrounding streetscape.

In the context of the Victoria Parade Precinct, the local landmarks are described in Table 2.

Table 2 – Local landmark buildings

Address	Building Name	Type	Corner	Grading	Photograph
61-65 Victoria Parade, Collingwood	Baden Powell Hotel	Hotel on a prominent corner site	Victoria Parade and Cambridge Street	Contributory (HO336)	



75-77 Victoria Parade, Collingwood	former John Franklin Hotel	Former hotel on a prominent corner site	Victoria Parade and Wellington Street	Individually Significant (HO336)	
135-141 Victoria Parade, Collingwood	Prince Patrick Hotel and shops	Hotel on a prominent corner site	Victoria Parade and Rokeby Street	Individual heritage place (HO138)	
139 Victoria Parade, Collingwood	Former Ebenezer Baptist Church	Place of worship	Victoria Parade and Rupert Street	Individual heritage place (HO139)	

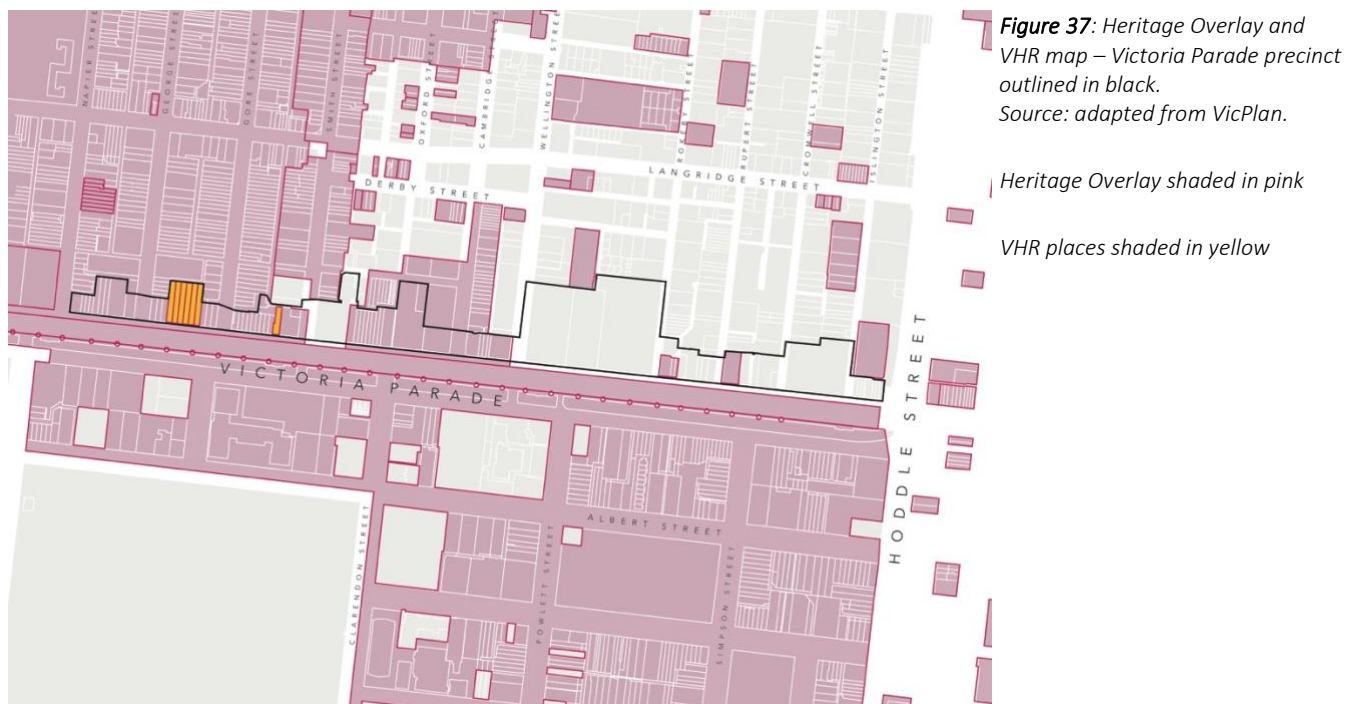
6.0 HERITAGE ANALYSIS

6.1 HERITAGE STATUS

6.1.1 Existing conditions

Approximately half of the length of the Victoria Parade Precinct is (intermittently) included within the extent of the Heritage Overlay. The western part of the precinct is subject to HO334 – South Fitzroy Precinct (between Napier and Smith streets) and HO336 – Victoria Parade Precinct (between Smith and Wellington streets) with two individual Heritage Overlays (HO138 – Prince Patrick Hotel and shops; HO139 – former Ebenezer Baptist Church) located in the eastern half of the precinct. Two places are included on the VHR namely Blanche Terrace at 169-179 Victoria Parade, Fitzroy (VHR H0177-H0182³) and McClelland Terrace at 203 Victoria Parade, Fitzroy (VHR H0590).

While not included within the study area, HO188 affects the street trees along the boulevard.



³ The individual listing of each of the six near-identical terraced houses that make up Blanche Terrace on the VHR is contrary to current practice and reflects the early registration of these buildings under the *Historic Buildings Act 1974*. It is also noted that the extent of registration includes no land (or curtilage) associated with these buildings. Under Heritage Council's longstanding practice these houses would now be considered as a single heritage place and would include the land on which they are located to the extent of the cadastral block.



Figure 38: Current heritage gradings from Appendix 8 – Victoria Parade precinct outlined in black. Source: adapted from VicPlan

The existing heritage status for the buildings within the study area are listed in Table 3. We note that there is some inconsistency between the street addresses provided within Appendix 8 and those identified in VicPlan. The addresses, dates of construction and gradings provided below are taken directly from Appendix 8.

Table 3 – Existing heritage status

VICTORIAN HERITAGE REGISTER				
<i>VHR #</i>	<i>Name</i>	<i>Address</i>	<i>Heritage Overlay</i>	<i>Date</i>
H177	Blanche Terrace	169 VICTORIA PARADE FITZROY	HO191	1867
H178	Blanche Terrace	171 VICTORIA PARADE FITZROY	HO192	1867
H179	Blanche Terrace	173 VICTORIA PARADE FITZROY	HO193	1867
H180	Blanche Terrace	175 VICTORIA PARADE FITZROY	HO194	1867
H181	Blanche Terrace	177 VICTORIA PARADE FITZROY	HO195	1867
H182	Blanche Terrace	179 VICTORIA PARADE FITZROY	HO196	1867
H590	McClelland. Terrace	203 VICTORIA PARADE	HO197	1882



INDIVIDUAL HERITAGE OVERLAYS

<i>Heritage Overlay #</i>	<i>Name</i>	<i>Address</i>	<i>Appendix 8 Grading</i>	<i>Date</i>
HO138	Prince Patrick Hotel and Shops	135-141 VICTORIA PARADE, COLLINGWOOD	Individually significant	1887
HO139	Former Ebenezer Baptist Church	159 VICTORIA PARADE, COLLINGWOOD	Individually significant	1870

PRECINCT HERITAGE OVERLAYS

<i>Heritage Overlay #</i>	<i>Name</i>	<i>Address</i>	<i>Appendix 8 Grading</i>	<i>Date</i>
HO334	South Fitzroy Precinct	OFFICES, 133-137 VICTORIA PARADE	Not contributory	1930 – 1940
		139-143 VICTORIA PARADE	Contributory	1860 - 1880
		ALEXANDRA TERRACE, 145-149 VICTORIA PARADE	Individually significant	1864-5
		SERVICE STATION, 151-159 VICTORIA PARADE	Not contributory	1950 - 1980
		163-165 VICTORIA PARADE	Individually significant	1868
		167 VICTORIA PARADE	Contributory	1850 - 1860
		RUSSELL HOUSE, 181 VICTORIA PARADE	Individually significant	1862
		OFFICES, 183 VICTORIA PARADE	Not contributory	1970 - 1980
		VICTORIA PEAK TOWNHOUSES, 187-201 VICTORIA PARADE	Not contributory	1970 - 1980
		SHOPS/OFFICES, 205-209 VICTORIA PARADE	Not contributory	1970 - 1980
		SHOPS/OFFICES, 1-3 SMITH STREET	Not contributory	1970 – 1980
		APARTMENTS, 4 GORE STREET	Not contributory	1970 - 1980
HO336	Victoria Parade Precinct, Collingwood	PORTIA, 15-17 VICTORIA PARADE	Individually significant	1889
		IRWELL TERRACE, 19-23 VICTORIA PARADE	Individually significant	1868



	25 VICTORIA PARADE	Contributory	1880 – 1890
	SHOP & RESIDENCE, 27-37 VICTORIA PARADE	Contributory	1880 - 1890
	FLORASTON, 39 VICTORIA PARADE	Individually significant	1876
	MELBOURNE AFTER CARE HOME, LATER AFTER CARE HOSPITAL, MELBOURNE DISTRICT NURSING SOCIETY, 45- 47 VICTORIA PARADE	Individually significant	1926, 1936
	SHOP & RESIDENCE, 49 VICTORIA PARADE	Contributory	1850- 1890
	SHOP & RESIDENCE, 51 VICTORIA PARADE	Contributory	1880- 1900
	SHOP & RESIDENCE, 53 VICTORIA PARADE	Contributory	1880- 1900
	55 VICTORIA PARADE	Contributory	1850- 1890
	57 VICTORIA PARADE	Contributory	1850- 1890
	SHOP & RESIDENCE, 59 VICTORIA PARADE	Contributory	1850- 1890
	BADEN POWELL HOTEL, 61-65 VICTORIA PARADE	Contributory	1880 – 1940
	67 VICTORIA PARADE	Contributory	1870 - 1890
Shop & residence	69 VICTORIA PARADE	Contributory	1870 - 1890
	71 VICTORIA PARADE	Contributory	1900 – 1915
	73 VICTORIA PARADE	Contributory	1900 – 1915
John Franklin Hotel, former	75A-77 VICTORIA PARADE	Individually significant	1880 - 1890

6.1.2 Recommended Changes

The review of the existing extent and grading of buildings was limited to substantial omissions and anomalies. The review did not include a complete re-assessment of



the 'Individually significant' / 'Contributory' / 'Not-contributory' gradings of individual buildings. A table of the identified anomalies is provided at Appendix I.

The row of Victorian-era shop/residences between Islington and Hoddle streets at the eastern end of the study area was identified as warranting inclusion on the Heritage Overlay as a small precinct and a citation has been prepared (see Appendix II).

It is noted that Blanche Terrace (VHR H0177, H0182) is included on the VHR as six individual buildings, when current practice would be to manage this as a single heritage place. Further, the Extent of Registration for Blanche Terrace (as gazetted in 1974) only includes the buildings themselves with no land or curtilage associated with the dwellings. While the land within each of these properties not occupied by a building or structure is also subject to HO334, this will not enable a holistic consideration of the heritage values of these properties when assessing permits under the *Planning and Environment Act 1987* or the *Heritage Act 2017*. We therefore recommend that an application be made under section 62(1) of the Heritage Act to amend the VHR entry to combine the properties into a single entry and include land to the extent of the cadastral blocks.

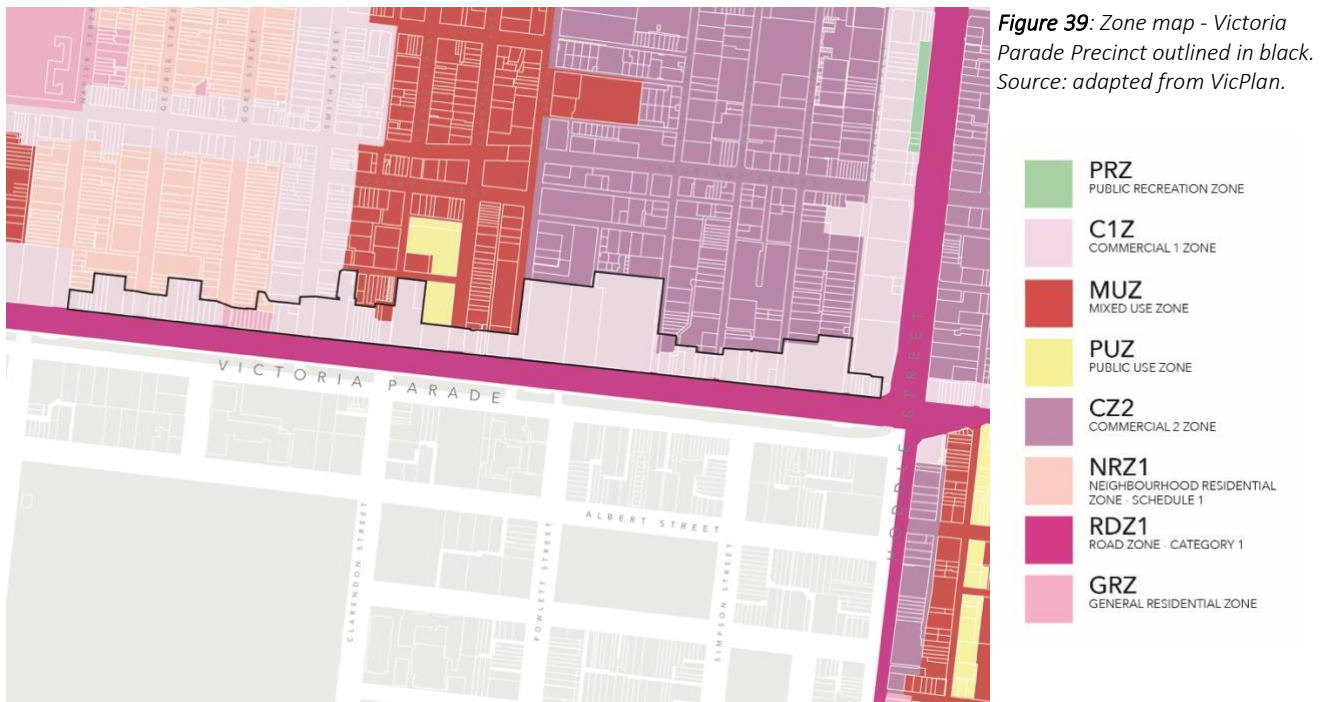
In addition, we recommend that the City of Yarra heritage grading map be updated to accurately reflect Appendix 8.

6.2 ZONING

6.2.1 Existing conditions

The carriageway and median strip of Victoria Parade, a major east-west thoroughfare, is zoned Road Zone (RDZ1). The land within the study area is generally zoned Commercial 1 Zone (C1Z). The land occupied by the Victoria Peak Townhouses (187-201 Victoria Parade, Collingwood) is General Residential Zone (GRZ). Small pockets on the northern edge of the study area are zoned Commercial 2 Zone (C2Z) (2-12 Rokeby Street, Collingwood), Mixed Use Zone (MUZ) (the rear of 29-35 Victoria Parade, Collingwood) and Neighbourhood Residential Zone (NRZ) (part 4 Gore Street).

The land to the north of the Victoria Parade Precinct within Fitzroy is generally zoned NRZ and is subject to HO334. Between Smith and Wellington streets the land north of the study area is generally zoned MUZ and is subject to HO336. East of Wellington Street the majority of the land north of the study area is zoned C2Z and is not subject to the Heritage Overlay.



6.2.2 Recommended Changes

The current zoning of land within the Victoria Parade Precinct is considered to be appropriate in heritage terms and no changes are recommended.

6.3 POTENTIAL FUTURE CHARACTER CONSIDERATIONS

The Victoria Parade Precinct has two principal built form characters. The first – found between Napier and Wellington streets to the west and Islington and Hoddle streets to the east – comprises fine grained, nineteenth century, two-storey terraced housing and shop/residences interspersed with later, low-rise residential and commercial development. The second character is typified by large format low- to medium-rise office, showroom and big-box retail buildings between Wellington and Islington Streets.

With the exception of the service station at 151-159 Victoria Parade, Fitzroy, the nineteenth century residential streetscape between Napier and Gore Streets is highly consistent in terms of building form (parapeted), setback (verandah with or without small garden), street wall height (two storey), materiality (rendered masonry) and architectural expression. While slightly less consistent in terms of building height and form, the buildings between the McDonalds Restaurant at the intersection of Victoria Parade and Smith Street to the west and Wellington Street to the east also have a high degree of integrity and visual cohesion. The row of shop/residences between Islington and Hoddle streets are largely intact, consistent in their age and architectural expression and have a high level of visual cohesion. In these three areas, with visual prominence of the consistent and cohesive heritage streetscapes should be retained and new development should remain recessive within the streetscape.



The large sites towards the eastern half of the study area have few sensitive interfaces and provide the opportunity for a significantly greater scale of development than within the intact heritage areas. Heritage considerations will have a more limited role in informing the overall preferred future character for this part of the Victoria Parade Precinct.

In relation to the individual heritage overlays (HO138 and HO139), future development proposals to these sites should consider the heritage values of that place and be sited, massed and designed to retain the visual prominence and three-dimensional form of these heritage places, namely: the Prince Patrick Hotel and shops (135-141 Victoria Parade, Collingwood) and the former Ebenezer Baptist Church (159 Victoria Street, Collingwood).

The Victoria Parade Precinct has sensitive heritage interfaces to the north between Gore and Little Smith streets (HO334) and between Smith and Wellington streets (HO336). New development should consider the impact on these heritage precincts in terms of visual and physical interface as well as amenity considerations

6.4 RECOMMENDED BUILT FORM PARAMETERS

Any DDO proposed for the Victoria Parade Precinct should apply built form guidance to ensure that new built form is respectful of the heritage places within the area.

A DDO control applied to properties within the Victoria Parade Precinct that are subject to – or immediately adjacent to – the Heritage Overlay should ensure new development respects the heritage significance of the graded buildings and is sited, massed and designed to be visually recessive and to not dominate the heritage place. This includes ensuring that appropriate interfaces are provided between the generally two-storey scale heritage buildings within heritage precincts and potential new development.

To develop appropriate built form parameters for the Victoria Parade Precinct, Hansen Partnership, the City of Yarra and GJM Heritage undertook the following analysis:

- Heritage and urban context information, known planning scheme amendments, and past and current planning applications was collected and analysed; and
- Built form parameters from the above analysis were drawn and then tested, firstly via cross-sectional drawings and then via a 3D model to determine their appropriateness.

Informed by the analysis and testing methodology outlined above, it is recommended that a DDO should seek built form outcomes that:

- Retain the visual prominence of local landmarks in the streetscape.
- Retain chimneys and principal roof forms visible from the public realm (excluding laneways).
- Ensure new development within the Heritage Overlay does not visually dominate the existing heritage fabric.

- Ensure any upper-level or infill development is subservient to heritage fabric and is visually recessive in mass, scale and materiality.
- Encourage the top-most level (or levels) of new development to be set further back from a principal heritage frontage (as encouraged at Clause 22.02-5.7.2) and treated as a visually separate roof top element.
- Retain the visual prominence of the return façades of buildings that address both Victoria Parade and cross streets (particularly at George Street (eastern side), Cambridge Street (both sides), Wellington Street (western side), Rokeby Street (eastern side) and Rupert Street (eastern side)) by setting back new upper-level built form from both street frontages.
- Establish a street wall height for infill development within consistent heritage streetscapes between Napier and Wellington streets that reflects the established two (Victorian-era) storey scale of those precincts.
- Encourage the ground level setback to match the lesser setback of any neighbouring heritage buildings.
- Ensure that the heritage buildings remain prominent within those parts of the Victoria Parade streetscape that are subject to the Heritage Overlay and retain their three-dimensional form as viewed from the public realm to avoid 'facadism'. This will require new upper-level development to be set back a minimum of 6m from the street wall and for redevelopment to respect the existing inter-floor heights of the heritage fabric.

7.0 BUILT FORM TESTING

To assist in the translation of the 'Recommended Built Form Parameters' in Part II into specific guidance that could be translated into a DDO control, the heritage analysis was reviewed against cross-sectional drawings of potential development envelopes and 3D computer modelling prepared by Hansen Partnership and the City of Yarra to test the appropriateness of particular built form outcomes that achieved the intent of the 'Recommended Built Form Parameters'.

As well as the cross-sectional drawing studies, 3D computer modelling using Sketchup Pro™ of potential bulk and massing envelopes for the study area was also interrogated. The existing built form was modelled along with approved, but not yet constructed, development in the Victoria Parade Precinct. It was used as a 'working' massing model to inform heights and setbacks on key development sites and to provide a comparative visual analysis. Given the relatively small proportion of the study area that is subject to the Heritage Overlay the 3D modelling was particularly useful in testing the generally more generous built form parameters applied to the larger, less-encumbered potential development sites rather than those with intact heritage buildings.

The Sketchup model was also interrogated to consider the impact of new buildings from the natural eye level (1.6m) on the public footpath.

Extensive field work was undertaken and site visits were used to inform the recommendations made in this report. Views of heritage places were only considered from the public footpath or from the central median strip with particular emphasis placed on intersections and tram stops where pedestrians are likely to dwell.

PART III: BUILT FORM RECOMMENDATIONS



8.0 BUILT FORM RECOMMENDATIONS

Any DDO applied to the Victoria Parade Precinct should include provisions to complement, but not duplicate, the decision guidelines at Clause 43.01-8 (Heritage Overlay), State Planning Policy at Clause 15.03-1S and local policy within Clauses 22.02, 22.03 and 22.10 (or as translated into the post-VC148 Planning Policy Framework through Amendment C269yara) of the Yarra Planning Scheme to inform new development.

Having regard to the heritage conditions within the study area as well as cross-sectional drawings and 3D modelling prepared by Hansen Partnership and the City of Yarra, it is recommended that the built form controls set out in 8.1 below be applied to new development to ensure appropriate weight is given to the heritage values within the study area. The cross-sectional drawings and 3D modelling helped assess whether or not the upper-level development would 'be visually recessive and not dominate the heritage place' as sought at Clause 22.02-5.7.1.

The cross-section diagrams and 3D modelling helped assess whether or not the upper-level development would 'be visually recessive and not dominate the heritage place' as sought at Clause 22.02-5.7.1 and proposed Clause 15.03-1L as endorsed by Council. While we note that the Panel considering C191yara did not consider a The use of mandatory upper-level setback controls and the application of a '45° angular plane' test (from Yarra DDO15) to protect the visual prominence of the street wall remains relevant tools for development behind the intact and cohesive heritage streetscapes between Napier and Gore streets, Smith and Wellington streets, and Islington and Hoddle Streets.

Buildings graded 'Individually Significant' and 'Contributory' or included in the VHR are referred to as 'heritage buildings' within the table below and those graded 'Not-contributory' or that are vacant are considered 'infill sites'. The Executive Director, Heritage Victoria will be the principal decision maker in relation to any redevelopment of the VHR places within the Victoria Parade Precinct⁴ and the controls identified below should be established to protect the heritage values of the precincts within which these State-listed heritage places are located.

8.1 RECOMMENDED DDO CONTROLS

Built Form Element	Mandatory	Preferred	Rationale
Street wall height (infill development) within or immediately adjacent to land	11m maximum 8m minimum	Match the parapet height of the adjacent heritage building to the width of the property boundary or	To ensure new built form responds to its immediate heritage context. A mandatory maximum and minimum street wall height is necessary to maintain

⁴ Blanche Terrace, 169-179 Victoria Parade, Fitzroy (VHR H0177-H0182) and McClelland Terrace, 203 Victoria Parade, Fitzroy (VHR H0590)

subject to the Heritage Overlay	-	for a distance of 6m whichever is less.	the two (Victorian-scale) storey form of the heritage streetscapes. The preferred control encourages new built form to match the height of the neighbouring heritage building.
Front setback (infill development) within or immediately adjacent to land subject to the Heritage Overlay	-	Match the setback, from any and all street frontages excluding laneway frontages, of the adjacent heritage building	To ensure new built form responds to the heritage context which generally has a small or no setback.
Minimum setbacks above street wall within or immediately adjacent to land subject to the Heritage Overlay	6m from the façade to Victoria Parade and Wellington Street Note: the setbacks for individual heritage places should be informed by their Statements of Significance and an analysis of the historic fabric of the heritage place.	6m from George, Cambridge, Rokeby, Rupert and Islington Streets 6m minimum for sites immediately abutting land subject to the Heritage Overlay	While the built form of heritage buildings varies across the Victoria Parade Precinct, there are largely intact heritage streetscapes within the precinct. A mandatory minimum 6m setback from the Victoria Parade and Wellington Street boundaries is appropriate given the highly consistent two-storey built form and the need to protect the visual prominence of the heritage street wall on these major thoroughfares. This distance will also retain the majority of visible chimneys and roof forms. The application of a mandatory 6m setback is consistent with that through DDO25, DDO26, DDO27 and DDO28 proposed to be introduced through C191yara. A larger setback from the principal facades of heritage buildings may be required development to be set back, and for the retention of an appropriate and legible three-dimensional heritage form. A preferred setback is appropriate from George, Cambridge, Rokeby, Rupert and Islington Streets to allow for the range of building forms and to recognise the more minor nature of these cross streets.
Maximum building heights within the Heritage Overlay	-	<u>Land subject to HO334</u> 11m (three storeys) (shallow sites) to 14m (four storeys) (deep sites) <u>Land subject to HO336</u>	The variety of site depths, existing urban form and interface conditions require a range of maximum building heights. Shallow sites are those less than range from approximately 25-30m in depth. Deep sides range from approximately 30m to 50m in depth.



14m (4 storeys) (shallow sites) to

21m (6 storeys) (deep sites)

Land subject to HO138
(135-141 Victoria Street, Collingwood)
14m (4 storeys)

Land subject to HO139
(159 Victoria Street, Collingwood)
11m (3 storeys)

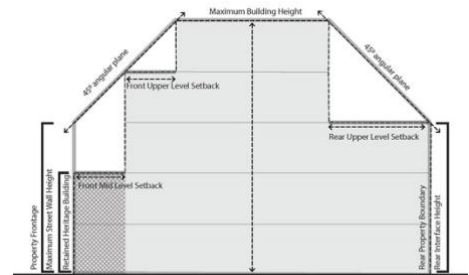
Note: there is limited opportunity for development on the former Baptist Church site beyond the rear car park area)

Land recommended for inclusion on the Heritage Overlay (205-219 Victoria Parade, Collingwood)
14m (4 storeys)

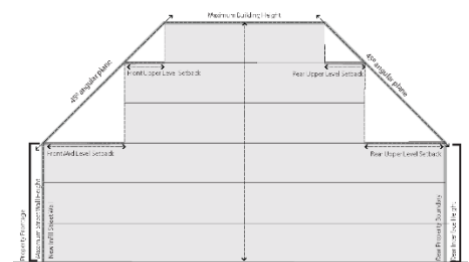
A preferred control will enable a wide range of design responses.

The overall height and building form should be informed by the preferred building envelope defined by the 45° angular plane described in DDO15.

Heritage Building



Infill building



8.2 ADDITIONAL GUIDANCE

The heritage policy proposed as part of C269yara includes specific strategies to address new development and alterations to commercial and industrial heritage places. If these provisions are implemented through Amendment C269yara, additional heritage design requirements are not likely to be needed within a DDO. However, if new development is to be informed by the existing heritage provisions at Clauses 15.03-1S, 21.05-1, 22.02 and 43.01 of the Yarra Planning Scheme, we recommend that the following heritage design requirements be applied to a DDO:

- New infill development within heritage precincts should:
 - Interpret the historic façade rhythm, including fenestration patterns and proportions, the relationship between solid and void, and the existing module of structural bays.
 - Retain the visual prominence of prominent corner buildings and local landmarks.
 - Be distinguishable from the original heritage fabric and adopt a high quality and respectful contextual design response.

- Ensure façade treatments and the articulation of new development are simple and do not compete with the heritage fabric.
- Avoid the replication of existing decorative features and architectural detail.
- The adaptation of existing heritage buildings should:
 - Discourage highly reflective glazing in historic openings.
 - Ensure the inter-floor height of the existing building is maintained and avoid new floor plates and walls cutting through historic openings.
 - Encourage the retention of solid built form behind retained facades and avoid balconies behind existing openings.
- New upper-level development behind existing heritage buildings should:
 - Retain the visual prominence of parapet and roof-top elements including parapets, balustrades, pediments, chimneys, lanterns, urns and other architectural features, where these exist.
 - Be set back to retain the visual prominence of prominent corner buildings and local landmarks.
 - Ensure that the design and setback of the addition does not visually dominate the heritage building or surrounding heritage places.
 - Retain the primacy of the three-dimensional form of the heritage building.
 - Incorporate materials and finishes that are recessive in texture and colour.
 - Generally utilise visually lightweight, but high quality, materials that create a juxtaposition with the heavier masonry of the heritage facades.
 - Incorporate simple architectural detailing so it does not detract from significant elements of the existing building or streetscape.
 - Provide a recessive backdrop to the heritage streetscape within precincts and to individual heritage buildings.
 - Avoid highly articulated facades with recessed and projecting elements.
 - Avoid highly contrasting or vibrant primary colours.
 - Avoid unarticulated façades that give a bulky appearance, especially from oblique views.
 - Be articulated to reflect the fine-grained character of narrow sites.
 - Encourage that upper-level development behind rows of identical or similar shop/residences is consistent in form, massing and façade treatment as existing upper-level development (where this exists).

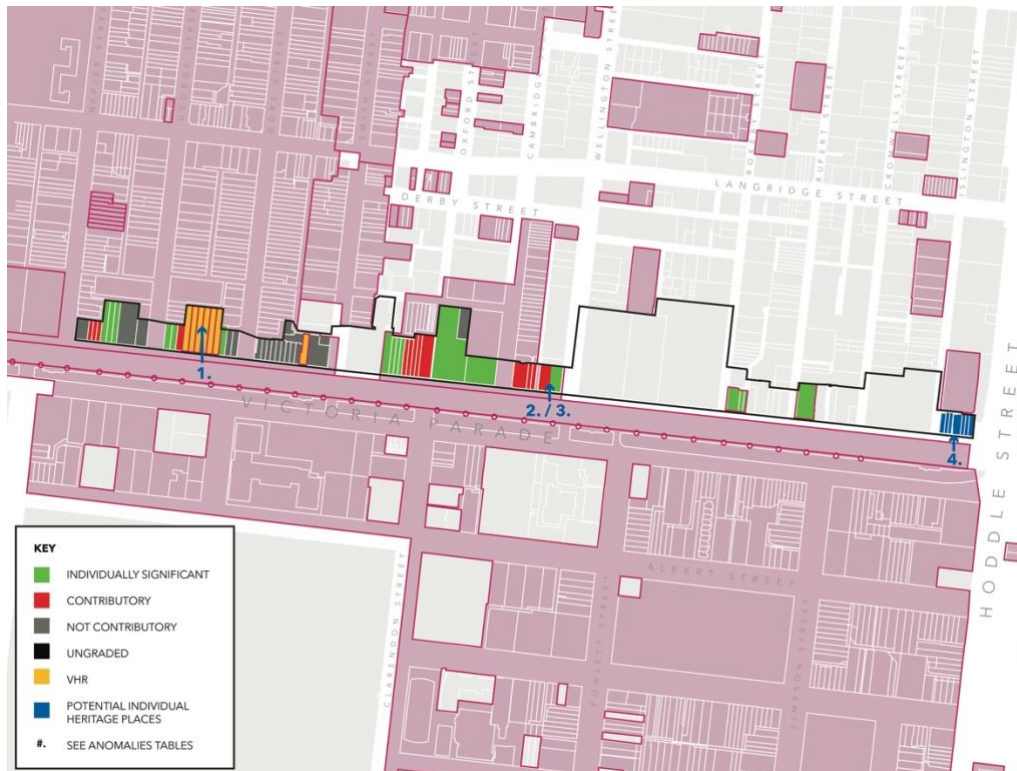
- New development on land immediately abutting heritage places should:
 - Provide a sensitive site-responsive transition between the existing heritage fabric and the new proposed built form.
 - Retain the visual prominence of prominent corner buildings and local landmarks.
 - Be distinguishable from the original heritage fabric and adopt a high quality and respectful contextual design response.
 - Ensure façade treatments and the articulation of new development are simple and do not compete with the heritage fabric.
 - Avoid the replication of existing decorative features and architectural detail.

APPENDIX I - Victoria Parade Precinct Anomalies

A1.1 Qualifications


- Only obvious omissions and anomalies have been identified
- Review of individual gradings across the precinct has not been undertaken
- Photos taken by GJM in October 2018 and March 2020 unless otherwise noted.

A1.2 Anomalies Map






A1.3 Anomalies

Recommendations are provided in **bold**.

MAP REF	ADDRESS (HO NUMBER)	CURRENT GRADING APPENDIX 8	CURRENT GRADING GRADING MAP	PROPOSED GRADING	COMMENTS / RECOMMENDATION (BOLD)	IMAGES
1.	<p>Blanch Terrace, 169-179 Victoria Parade, Fitzroy</p> <p>169 Victoria Parade (VHR H0177; HO191)</p> <p>171 Victoria Parade (VHR H0178; HO192)</p> <p>173 Victoria Parade (VHR H0179; HO193)</p> <p>175 Victoria Parade (VHR H0180; HO194)</p> <p>177 Victoria Parade (VHR H0181; HO195)</p> <p>179 Victoria Parade (VHR H0182; HO196)</p>	VHR	Individually significant	VHR (single heritage place)	<p>This terrace of six houses dating from 1867 are included on the VHR as six separate entries. No land is associated with the registration meaning only the land itself is included on the VHR. Blanch Terrace, a whole is a heritage place and a single VHR entry with associated land should be applied to the heritage place. It is recommended that application be made under s.62(1) of the Heritage Act 2017 to amend the VHR entry.</p> <p>Make application to amend the extent to include land associated with the buildings and combine VHR H0177, H0178, H0178, H0179, H0180, H0181 & H0182.</p>	



2.	71 Victoria Parade, Collingwood (HO336)	Contributory	Individually significant	Contributory	There is an inconsistency in grading between Appendix 8 and the grading map. Update grading map to reflect Appendix 8 grading.	
3.	73 Victoria Parade, Collingwood (HO336)	Contributory	Individually significant	Contributory	There is an inconsistency in grading between Appendix 8 and the grading map. Update grading map to reflect Appendix 8 grading.	

4.	205-219 Victoria Parade, Collingwood	No HO	No HO	Potential new HO precinct	<p>Numbers 205, 207, 209, 211-13, 215 and 217 Victoria Street, Collingwood are an intact but small row of single and two-storey shop residences not included on the Heritage Overlay.</p> <p>The heritage assessment prepared by GJM identified that they are of local significance for their historical (Criterion A) and representational (Criterion D) values.</p> <p>Include 205-217 Victoria Parade, Collingwood on the Schedule to the Heritage Overlay</p>	
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**APPENDIX II – Citations and Statement of Significance:
Victoria Parade East Precinct, 205-217 Victoria Parade,
Collingwood**



VICTORIA PARADE EAST PRECINCT

205-219 VICTORIA PARADE, COLLINGWOOD, 3066



205-219 Victoria (GJM Heritage, March 2020).

DATE: 27 August 2020 - DRAFT

FILE: 2020-008

VICTORIA PARADE EAST PRECINCT

205-219 VICTORIA PARADE, COLLINGWOOD

Place type: Commercial shops & residences	Architect: Not known
Construction Date: Victorian period	Builder: Not known
Recommendation: Include in the Heritage Overlay as a heritage precinct comprising seven (7) 'Contributory' buildings	Extent of Overlay: To the extent of the property boundaries including the rear laneway. Refer to the plan at Figure 13.

Precinct History

Commercial development within the Victoria Parade East Precinct commenced in the 1860s, with three timber shops built by 1870 (S&Mc; RB). The Sands & McDougall Directories and Rate Books appear to indicate that the current structures at 211 and 219 Victoria Parade were the first brick buildings constructed in the row. They were constructed in the early-to-mid 1870s for Daniel and Richard Roberts to replace earlier timber buildings in these locations (no. 211 appears to be incorrectly shown as a timber building in the 1897 Melbourne and Metropolitan Board of Works [MMBW] plan at Figure 1). In the 1870s, other occupants of the row included a general dealer and fruiterer and greengrocer (in timber shops), and a timber and stone workshop at the corner of Islington Street. At the far east end of the block, on the corner of Hoddle Street (outside of the precinct, since demolished), was the Junction Hotel that was established in the early 1870s.

The remainder of the row was constructed in the 1880s, with the rate books listing seven brick shops within the precinct by 1884. These shops were built for three owners – the aforementioned Daniel Roberts (current 215-219) and Richard Roberts (current 211-213), and William Pearce (current 205-209) (RB). The property at 211-213 Victoria Parade, owned and occupied by Richard Roberts, bootmaker, was addressed as one property at this time (no. 205). The Net Annual Value of Roberts' property increased in the early 1880s, suggesting works to the property. In the 1880s, occupants within the precinct included a haberdasher, milliner, bootmaker, leather seller, plumber, tobacconist and furniture broker (S&Mc).

The 1897 and 1899 MMBW plans (Figures 1 and 2) show the developed commercial strip, and the original extent of the block before the eastern section was demolished in the 1970s to make way for the expansion of Hoddle Street. The 1899 plan shows the earlier address numbers, with the buildings at the current 205-219 Victoria Parade addressed as 199-213, with no shopfront verandahs to the row. The 1897 plan suggests that the current 211 Victoria Parade was constructed of timber at that date (with horizontal hatching), however the rate books record the building as brick.

Between 1907 and 1910 the numbering along Victoria Parade changed, establishing the current address numbers. Occupants of the commercial row in 1905 comprised a hairdresser and tobacconist, bookseller, bootmaker, leather dealer, news agent and stationer, confectioner and plumber. The strip retained its commercial character throughout the twentieth century. In the early 1970s the buildings east of the precinct were demolished to facilitate the widening of Hoddle Street (S&Mc).



Figure 1. The commercial strip in 1897. The extent of the precinct is indicated in green. Diagonal hatching indicates a brick or stone building, while the horizontal hatching indicates a building constructed of wood (MMBW No 28, dated 1897)

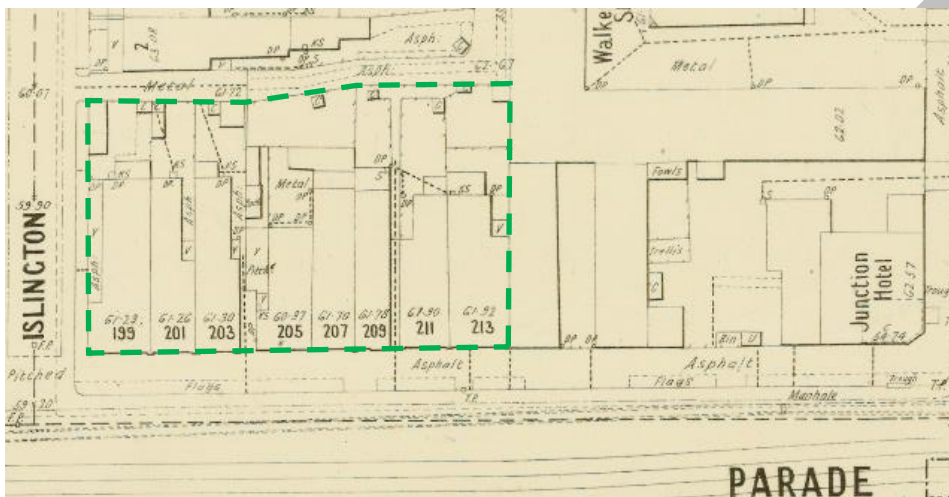


Figure 2. The commercial row in 1899. The extent of the precinct is indicated in green. Nos. 199 to 213 shown are the current 205-219 Victoria Parade (MMBW DP No. 1210, dated 1899)

Historical Themes

The place illustrates the following themes as outlined in the *City of Yarra Thematic History* (1998):

- 4.0 Developing local economy
 - 4.4 Smaller Retailers: Strip Shopping

Description

The row of commercial buildings at 205-219 Victoria Parade is in the inner-northern suburb of Collingwood on the north side of Victoria Parade and to the west of the intersection with Hoddle Street. The row is bordered by Islington Street to the west and a bluestone laneway to the north. A small grassed reserve to the east separates the row from Hoddle Street. The buildings have no front or side setbacks but a narrow pedestrian laneway separates nos. 209 and 211. All buildings are of brick construction and have rendered facades with roofs set behind parapets.



205

207-209

211-213

215-219



Figure 3. Victoria Parade elevations and aerial photograph (aerial photograph source: Nearmap)

205 and 207-209 Victoria Parade

205-209 Victoria Parade comprises three shops constructed as a group. 205 is a two-storey shop-residence, and 207-209 are two single-storey shops. The shops are of rendered masonry construction, each with a hipped roof clad in corrugated sheet metal concealed behind a parapet featuring semicircular pediments to each address. The upper façade of no. 205 contains two timber-framed sliding sash windows with moulded

surrounds, and the ground floor façade has been modified through the introduction of shopfront glazing, which flank the central recessed entry. At 207-209, large shopfront glazing is also present, and the recessed entrance to each of 207-209 is adjacent to the party wall the properties share. The party walls featured vermiculated semicircular corbels. Lions heads are present above these at the central and eastern party walls. A narrow easement is located immediately to the east of the building, and a later two-storey addition has been constructed to the rear of no. 205.

211-213 Victoria Parade

211-213 Victoria Parade comprises a single-storey shop with canopy to the street. The building is a rendered masonry structure with two hipped-roof volumes clad in corrugated sheet metal concealed by a low parapet with dogtooth brick course and a simple central pediment. The western hip of the roof is wider than the eastern hip and has a corbeled chimney with cylindrical terracotta pot. The street façade is punctuated with large expanses of timber-framed shopfront glazing. The entrance is centrally located with a half-glazed four-panel timber door. The verandah is a later addition and comprises a central barrel-vault over the entry, which is clad in painted corrugated metal, with concave sections to either side and decorative cast iron valances and brackets. The rear of the site is at-grade carpark, accessed from the rear laneway via Islington Street. A narrow easement is located immediately to the west of the building.

215-219 Victoria Parade

215-219 Victoria Parade comprises a row of three two-storey shop-residences. The buildings are of rendered masonry construction with the three facades exhibiting the same detailing to the parapets and window openings and surrounds. Party walls are articulated at the upper and lower string course by moulded corbels featuring a Queen's head motif. One of four urns to the parapet remains extant, and each shop features its own pediment. Three rendered brick chimneys remain extant – two at the east elevation of 219 and one to the west elevation of 215.

The upper-storey windows of each shop have identical moulded surrounds, with one central window to each of nos. 215 & 217 and a pair of centrally positioned windows to no. 219. Nos. 215 & 217 contain non-original timber-framed windows and no. 219 appears to retain the original timber-framed sliding sash windows. The shopfronts at ground floor level have been modified through the introduction of shopfront glazing. The recessed entries at nos. 215 & 217 are located to the east of the shopfront window while the entry to no. 219 is centrally located. An additional door is located to the east of the main entry of no. 215 and comprises a later timber door with fanlight above. Various single-storey structures are located to the rear of the building.



Figure 4. The precinct as viewed from the road reserve at the centre of Victoria Parade, looking northeast. Islington Street at left.



Figure 5. Looking northeast to 211-213 Victoria Parade, with canopy at front, and 215-219 Victoria Parade at right.



Figure 6. 207-209 Victoria Parade, showing extant parapets and lion's head moulding to party wall.



Figure 7. Looking east to the canopy at 211-213, showing later steel structure above cast iron verandah posts.



Figure 8. Looking southwest to the rear of 215-219 Victoria Parade, showing extant chimneys to the east elevation and single-storey outbuildings (at right).

Integrity/Intactness

The buildings retain a high degree of integrity to the Victorian period in fabric, form and detail. While the buildings have undergone some alterations, including the replacement of windows and doors, the loss of shopfronts at street level, the loss of some decorative detailing, and the construction of a verandah canopy at no. 211-213, these do not diminish the ability to understand and appreciate the precinct as a highly intact row of Victorian commercial premises.

Comparisons

The row of commercial premises at 205-219 Victoria Parade, Collingwood are of note as an intact and representative row of Victorian-era commercial buildings, constructed in the City of Yarra.

Substantial numbers of commercial buildings were constructed in the City of Yarra in the 1870s and 1880s. Those of the late nineteenth century typically incorporated classical elements and motifs into symmetrical parapeted facades with varying degrees of elaboration. Rendered facades were most common, with some facades of red brick and contrasting render and others of polychromatic brickwork. Rendered decoration, in the form of classical elements such as cornices, architraves, balustrading, urns, stringcourses and pediments, was commonly applied to facades. Windows were typically rectangular, sometimes arch-headed, and these were repeated regularly across upper facades.

Commercial buildings from this period were most commonly of two-storeys, with some single-storey and three or four-storey premises constructed. They were typically built as rows of attached buildings, as pairs, or as individual buildings with no side setbacks from adjoining properties and no front setback.

Within the City of Yarra, large numbers of commercial premises built in the Victorian periods are included in the Heritage Overlay as individually significant and contributory places within precincts. Comparative groups of buildings within existing, larger precincts, which broadly display characteristics similar to the precinct at 205-219 Victoria Parade, include amongst others:

- 409-417 Swan Street, Richmond (Burnley Street Precinct, HO474)

- 91-101 Swan Street, Richmond (Swan Street Precinct, HO335)
- 129-141 Bridge Road, Richmond (Bridge Road Precinct, HO310)
- 178-186 Bridge Road, Richmond (Bridge Road Precinct, HO310)
- 97-135 Church Street, Richmond (Church Street North Precinct, HO454)
- 233-251 Victoria Street, Abbotsford (Victoria Street West Precinct, HO444).

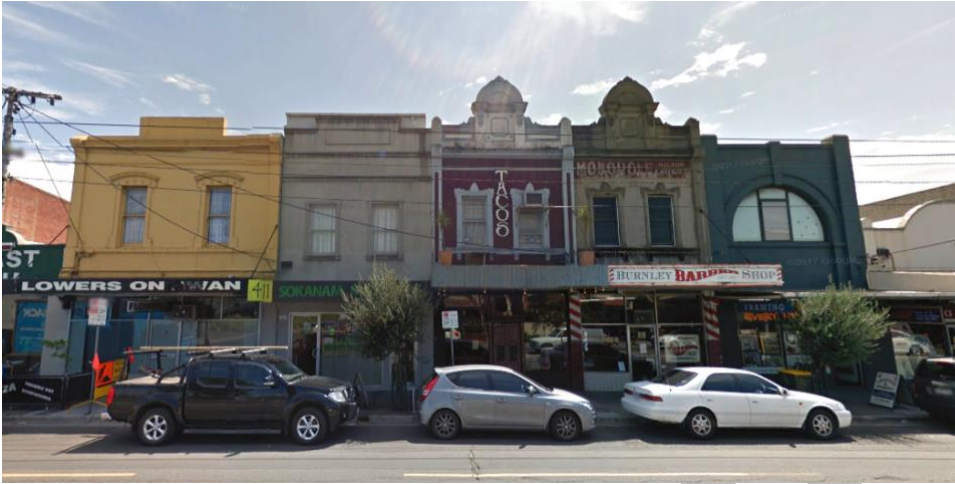


Figure 9. 409-417 Swan Street, Richmond (Burnley Street Precinct, HO474) (Google, Feb 2017).



Figure 10. 91-101 Swan Street, Richmond (Swan Street Precinct, HO335) (Google, Oct 2016).



Figure 11. 129-141 Bridge Road, Richmond (Bridge Road Precinct, HO310) (Google, Nov 2016).



Figure 12. 178-186 Bridge Road, Richmond (Bridge Road Precinct, HO310) (Google, Oct 2016).



Figure 13. 97-135 Church Street, Richmond (Church Street North Precinct, HO454) (Google, Oct 2016)

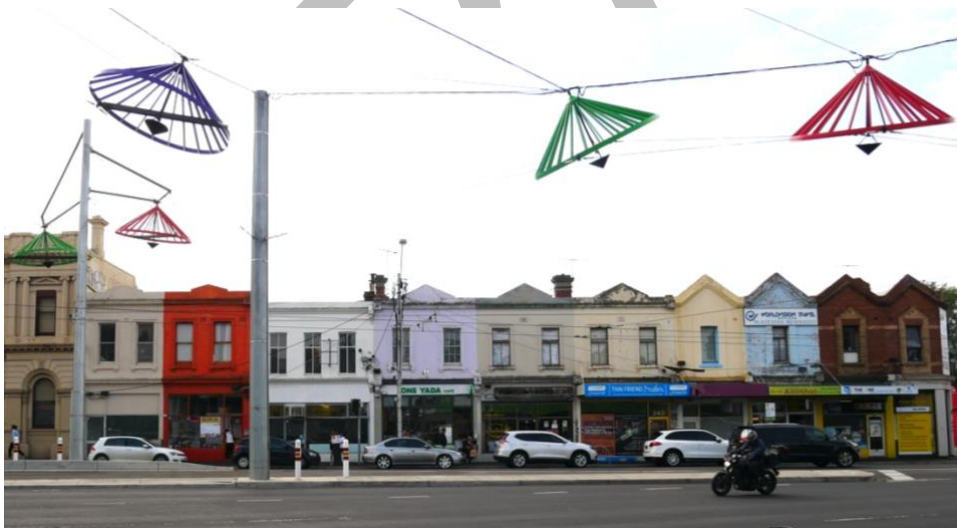
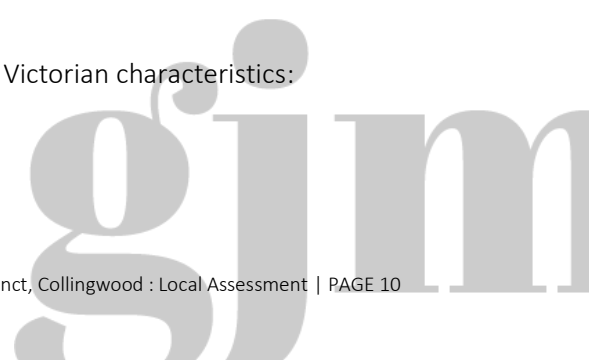


Figure 14. 233-251 Victoria Street, Abbotsford (Victoria Street West Precinct, HO444) (GJM, September 2017).

Like these places, the row at 205-219 Victoria Parade, Collingwood displays a range of characteristics which have strong associations with the Victorian period and the precinct remain highly intact to demonstrate these associations.

The commercial row at 205-219 Victoria Parade demonstrate the following Victorian characteristics:

- A variety of simple facade parapets, with pitched roofs behind;
- No front setbacks;



- Rendered walls;
- Rendered window frames, sills and hoods to upper storeys;
- Horizontal lines formed by parapets, cornices, and string courses; and
- Repetitive upper floor fenestration patterns.

Assessment Against Criteria

Following is an assessment of the place against the recognised heritage criteria set out in *Planning Practice Note 1: Applying the Heritage Overlay* (August 2018).

Criterion A: Importance to the course or pattern of our cultural or natural history

The Victoria Parade East Precinct, comprising 205-219 Victoria Parade, Collingwood, is illustrative of the historical development that occurred along a major, early commercial thoroughfare in the City of Yarra, particularly in the ‘boom’ period of the 1880s.

Criterion D: Importance in demonstrating the principal characteristics of a class of cultural or natural places or environments (representativeness)

The Victoria Parade East Precinct, comprising 205-219 Victoria Street, Collingwood is an intact, representative example of a row of Victorian commercial premises. The buildings clearly demonstrate the principal characteristics of nineteenth century development found along major thoroughfares within the City of Yarra and display typical features of the Victorian architectural style popular in Collingwood and across Melbourne more broadly, including parapeted facades with repetitive upper floor fenestration, rendered facades with decorative moulding and ground floor shopfronts.

Grading and Recommendations

It is recommended that the precinct be included in the Heritage Overlay of the Yarra Planning Scheme as a heritage place comprising seven ‘Contributory’ buildings.

Recommendations for the Schedule to the Heritage Overlay (Clause 43.01) in the Yarra Planning Scheme:

External Paint Controls?	No
Internal Alteration Controls?	No
Tree Controls?	No
Outbuildings or Fences not exempt under Clause 43.01-3?	No
Prohibited Uses Permitted?	No
Aboriginal Heritage Place?	No

Extent of the Recommended Heritage Overlay

To the property title boundaries, as indicated by the polygon on the aerial below.

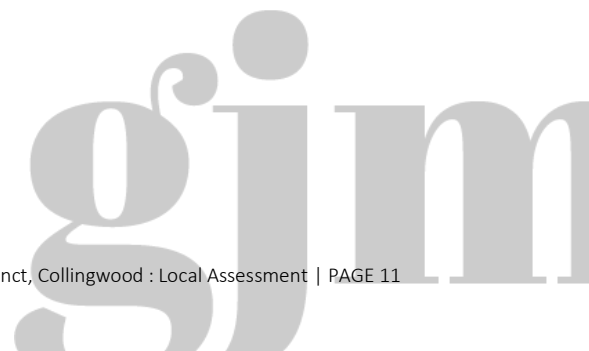




Figure 15. Recommended Extent of Heritage Overlay
(Basemap Source: Nearmap, June 2020)

References

Hodgkinson, Clement, Noone, John, and Wilkinson, John, (1858), Plan shewing the streets and buildings in existence in East Collingwood on January 1st 1858 : with schedule of heights of bench-marks above low water datum at Queen's wharf [cartographic material]. Printed by De Gruchy & Leigh [Melbourne].

Melbourne and Metropolitan Board of Works (MMBW): Map No 28, dated 1897; Detail Plan No. 1210, dated 1899.

Sands & McDougall Directories (S&MC).

Rate Books (RB): Borough, then City of Collingwood, Barkly Ward, South Glasshouse Division: 1864, entries 137-140; 1870, entries 157-161; 1876, entries 172-177; 1879, entries 178-182; 1882, entries 178-182; 1883, entries 176-182; 1884, entries 181-187; 1885, entries 182-188; 1888, entries 234-241; 1893, entries 235-243; 1898-1900, entries 225-233.

DRAFT

gjim

Victoria Parade East Precinct, 205-219 Victoria Parade, Collingwood Statement of Significance, August 2020 [DRAFT]

Heritage place: Victoria Parade East Precinct,
205-219 Victoria Parade, Collingwood, 3066

PS ref no.: HO TBC



What is significant?

The terraced row of late nineteenth century shop residences at 205-219 Victoria Parade, Collingwood.

Elements that contribute to the significance of the place include (but are not limited to):

- The terraced row's form, materials and detailing from its Victorian era period of construction, including its Victoria Parade elevation, fenestration, decorative parapets, mouldings and other details.
- Original roof forms and chimneys.
- Cast iron elements of the verandah to nos. 211-213 Victoria Parade.

The later (twentieth century) additions including rear additions, later shopfront glazing and the steel structure above the cast iron verandah posts to nos. 211-213 Victoria Parade are not significant.

How is it significant?

The Victoria Parade East Precinct at 205-219 Victoria Street is of local historical and architectural (representative) significance to the City of Yarra.

Why is it significant?

The Victoria Parade East Precinct, comprising 205-219 Victoria Parade, Collingwood, is illustrative of the historical development that occurred along a major, early commercial thoroughfare in the City of Yarra, particularly in the 'boom' period of the 1880s (Criterion A).

The Victoria Parade East Precinct, comprising 205-219 Victoria Street, Collingwood is an intact, representative example of a row of Victorian commercial premises. The buildings clearly demonstrate the principal characteristics of nineteenth century development found along major thoroughfares within the City of Yarra

and display typical features of the Victorian architectural style popular in Collingwood and across Melbourne more broadly, including parapeted facades with repetitive upper floor fenestration, rendered facades with decorative moulding and ground floor shopfronts (Criterion D).

Primary source:

Victoria Parade: Heritage Analysis and Recommendations, GJM Heritage, August 2020

Draft